



COVINA TOWN CENTER SPECIFIC PLAN

ADOPTED | NOVEMBER 19, 2019



ACKNOWLEDGEMENTS

The following individuals and organizations contributed to the preparation of the Covina Town Center Specific Plan.
Funding provided by Metro, a partner in our community.



CITY COUNCIL

John C. King, Mayor
Victor Linares, Mayor Pro Tem
Walter Allen, III, Council Member
Patricia Cortez, Council Member
Jorge A. Marquez, Council Member

PLANNING COMMISSION

Brad Manning, Chairperson
Charles Hodapp
John Connors
Dan McMeekin
Win Patterson

CITY STAFF

Brian Saeki, City Manager
Brian K. Lee, AICP, Director of Community
Development

CONSULTANTS

MIG, Inc.
Planning Lead and Community Engagement
Nelson|Nygaard Consulting Associates
Transportation Planning
Leland Consulting Group
Market Analysis
Nancy Fong, AICP
City of Covina Project Manager

COVINA TOWN CENTER SPECIFIC PLAN

NOVEMBER 19, 2019
RESOLUTION CC 19-114



CITY OF COVINA

125 E. College Street
Covina, CA 91723

Contact: Brian Lee, Community Development Director

Prepared by:



537 S. Raymond Avenue
Pasadena, CA 91105

In association with:

LELAND CONSULTING GROUP
NELSON|NYGAARDCONSULTING ASSOCIATES



Metro

Partnering
with your
community

Amended (Date)	Resolution
9-01-2020	CC 2020-80
9-20-2022	CC 2022-99
4-18-2023	CC 2023-23

COVINA TOWN CENTER SPECIFIC PLAN

TABLE OF CONTENTS

CHAPTER 1 INTRODUCTION

Key Concepts.....	1-4
Purpose.....	1-5
Plan Context.....	1-6
Specific Plan Process.....	1-8
Community Engagement.....	1-8
How to use this Plan.....	1-9
Document Overview.....	1-10
Environmental Review.....	1-11

CHAPTER 2 VISION, FRAMEWORK, AND STRATEGIES

Vision.....	2-3
Vision Elements.....	2-3
Town Center Districts & Strategies.....	2-5
Town Center Districts.....	2-6

CHAPTER 3 LAND USE AND ZONING

Land Use Designations.....	3-4
Area Guidelines and Standards.....	3-9
Allowable Land Use by Zoning Designation.....	3-20

CHAPTER 4 DESIGN GUIDELINES AND STANDARDS

Introduction.....	4-3
Purpose.....	4-4
Public Realm Design Standards and Guidelines.....	4-5
Private Realm Design Guidelines and Standards.....	4-28

CHAPTER 5 MOBILITY AND INFRASTRUCTURE

Introduction.....	5-3
Overarching Objectives and Vision.....	5-4
Street Network.....	5-5
Covina Metrolink Station Concept.....	5-6
Citrus Avenue Concept.....	5-9
North 2 nd Avenue Concept.....	5-14
East Front Street Concept.....	5-16
Health Corridor Concept.....	5-18
Pedestrian.....	5-21
Bicycle.....	5-22
Transit.....	5-23
Parking Management.....	5-25
Water Infrastructure.....	5-28
Sanitary Sewer Infrastructure.....	5-30
Storm Drainage Infrastructure.....	5-32
Dry Utilities Infrastructure.....	5-35

CHAPTER 6 IMPLEMENTATION

Overarching Objectives.....	6-4
Specific Plan Administration.....	6-5
Non-Conforming Uses and Buildings.....	6-7
Funding Sources.....	6-9
Implementation Action Plan.....	6-14

CHAPTER 7 GLOSSARY

Eating, Entertainment, and Drinking Establishments...	7-3
Industrial.....	7-5
Institutions and Public Facilities.....	7-6
Medical-Related Uses.....	7-7
Mixed Uses.....	7-8
Recreation and Public Assembly Uses.....	7-8
Residential.....	7-9
Retail Uses.....	7-10
Service Uses.....	7-12
Visitor Accommodations.....	7-13
Floor Area (Gross).....	7-13
Floor Area Ratio.....	7-14

TABLES

Table 3-1: Land Use Designations.....	3-5
Table 3-2: Allowable Land Uses.....	3-22
Table 4-1: Building Form.....	4-29
Table 4-2: Parking Requirements.....	4-30

FIGURES

Figure 1-1: Planning Area.....	1-7
Figure 1-2: Community Engagement.....	1-8
Figure 2-1: Town Center Districts.....	2-6
Figure 2-2: Front Street/TOD/F.A.I.R map.....	2-7
Figure 2-3: Front Street/TOD/F.A.I.R.....	2-9
Figure 2-4: Citrus Ave District map.....	2-10
Figure 2-5: Citrus Ave District	2-12
Figure 2-6: Downtown West District map.....	2-13
Figure 2-7: Downtown West District.....	2-15
Figure 2-8: Civic and Cultural Core District map.....	2-16
Figure 2-9: Civic and Cultural Core.....	2-18
Figure 2-10: Center Street Neighborhood District.....	2-19

Figure 3-1: Land Use Designations.....	3-4	Figure 4-11: Public Realm Signs Standards.....	4-18
Figure 3-2: Calculating FAR.....	3-5	Figure 4-12: Plaza Design/Community Activity Area I.....	4-20
Figure 3-3: Residential Town Center West Zone.....	3-9	Figure 4-13: Plaza Design/Community Activity Area II.....	4-21
Figure 3-4: Residential Town Center East Zone.....	3-10	Figure 4-14: Pedestrian Alleyway.....	4-23
Figure 3-5: Neighborhood Zone.....	3-11	Figure 4-15: West Cottage Drive and West College Street Health Corridors.....	4-25
Figure 3-6: Transit Oriented Development/ High Density Residential Zone.....	3-12	Figure 4-16: Green Infrastructure.....	4-27
Figure 3-7: Mixed-Use Zone.....	3-14	Figure 4-17: Setbacks and Stepbacks Standards.....	4-33
Figure 3-8: Food Arts Industrial Residential (F.A.I.R.) Zone.....	3-16	Figure 4-18: Mixed Use and Nonresidential Site Design Standards.....	4-36
Figure 3-9: Historic Core Zone.....	3-17	Figure 4-19: Building Façades I.....	4-39
Figure 3-10: Civic Zone.....	3-18	Figure 4-20: Building Façades II.....	4-40
Figure 3-11: Cultural Core Zone.....	3-19	Figure 4-21: Building Façades III.....	4-41
Figure 3-12: Medical Core Zone.....	3-20	Figure 4-22: Ground Floor Design.....	4-43
Figure 3-13: Rail Zone.....	3-21	Figure 4-23: Industrial, Stand-Alone Commercial, Horizontal Mixed-Use.....	4-45
Figure 4-1: Sidewalk Paving.....	4-5	Figure 4-24: Vertical Mixed-Use Development I.....	4-47
Figure 4-2: Crosswalks and Crosswalk Treatment Standard.....	4-6	Figure 4-25: Vertical Mixed-Use Development II.....	4-48
Figure 4-3: Street Furniture and Lighting Standards Citrus Avenue District.....	4-8	Figure 4-26: F.A.I.R. Incubator Space Development.....	4-50
Figure 4-4: Street Furniture and Lighting Standards Cultural Core.....	4-9	Figure 4-27: F.A.I.R. Incubator Space Development.....	4-51
Figure 4-5: Street Furniture and Lighting Standards Front Street/TOD/F.A.I.R. District.....	4-10	Figure 4-28: Entries, Doors, and Windows I.....	4-52
Figure 4-6: Gateways (map).....	4-11	Figure 4-29: Entries, Doors, and Windows II.....	4-53
Figure 4-7: Gateways.....	4-13	Figure 4-30: Entries, Doors, and Windows III.....	4-54
Figure 4-8: Public Art Guidelines.....	4-14	Figure 4-31: Architectural Projections Standards Materials and Finishes Guidelines.....	4-56
Figure 4-9: Curb Extensions (Bulb-outs).....	4-15		
Figure 4-10: Parklets.....	4-17		

Figure 4-32: Architectural Projections Standards			
Materials and Finishes Guidelines.....	4-57	Figure 5-13: Citrus Ave Food Hall Concepts I.....	5-13
Figure 4-33: Lighting.....	4-59	Figure 5-13: Citrus Ave Food Hall Concepts II.....	5-13
Figure 4-34: Signs.....	4-61	Figure 5-13: Citrus Ave Food Hall Concepts III.....	5-13
Figure 4-35: Awnings.....	4-63	Figure 5-13: Citrus Ave Food Hall Concepts IV.....	5-13
Figure 4-36: Accessory Structures Standards.....	4-64	Figure 5-11: North 2 nd Avenue Sample Cross.....	5-14
Figure 4-37: Building/Structure		Figure 5-12: North 2 nd Avenue Enhancements.....	5-15
Enhancements at Key Intersections I.....	4-65	Figure 5-13: East Front Street/North 2 nd	
Figure 4-38: Building/Structure		Avenue Enhancements.....	5-17
Enhancements at Key Intersections II.....	4-66	Figure 5-14: Health Corridor Concept:	
Multifamily Residential, Commercial,		North 3 rd Avenue, West Cottage Drive,	
Industrial, and Mixed-Use Parking		and West College Street.....	5-18
Figure 4-39: Facilities Guidelines.....	4-69	Figure 5-15: Health Corridor Enhancements.....	5-19
Figure 4-40: Parking Structure Guidelines I.....	4-71	Figure 5-16: East San Bernardino Road	
Figure 4-41: Parking Structure Guidelines II.....	4-72	Buffered Bike Lane – Sample Cross-Section.....	5-20
Figure 4-42: Uncovered Parking Area		Figure 5-17: Improved Bus Boarding Zones.....	5-24
Landscaping Standards and Guidelines.....	4-73	Figure 5-18: Improved Bus Stops.....	5-24
Figure 4-43: Sustainability I.....	4-75	Figure 5-19: Covina Metrolink Station Images.....	5-25
Figure 4-44: Sustainability II.....	4-76	Figure 5-20: Wayfinding Images.....	5-26
Figure 5-1: North Citrus Avenue,		Figure 5-21: Existing Parking Structure Images.....	5-27
East Front Street, and Covina Metrolink		Figure 5-22: Water Lines Map.....	5-29
Station Enhancements.....	5-6	Figure 5-23: Sewer Lines Map.....	5-31
Figure 5-2: Streetscape Improvements.....	5-8	Figure 5-24: Existing Parking Structure Images.....	5-33
Figure 5-3: Curb Extension/Bulb-out Images.....	5-10	Figure 5-25: Storm Drain Map.....	5-34
Figure 5-4: North Citrus Avenue/San Bernardino		Figure 7-1: Floor Area Ratio.....	7-14
Road Pedestrian Enhancements.....	5-11		
Figure 5-5: North Citrus Avenue/Badillo Street			
Pedestrian and Bicycle Enhancements.....	5-11		
Figure 5-6: Mid-Block Crossing Images.....	5-12		

COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 1: INTRODUCTION





Chapter 1 Contents

- Key Concepts
- Purpose
- Plan Context
- Specific Plan Process
- Community Engagement
- How to use this Plan
- Document Overview
- Environmental Review

CHAPTER 1: INTRODUCTION

INTRODUCTION

Covina is located within a beautiful valley setting and is home to an array of assets and amenities. From its historic Downtown and expanding medical core, to its rich history in the citrus industry, Covina is a unique community that evokes a sense of pride in its residents.

Although Covina has a beautiful Downtown, it lacks connectivity to the surrounding community and transit center. This lack of connectivity limits exposure to assets and amenities available in the surrounding area and region.

As a result, in the early 2000s, the City embarked on a Town Center Specific Plan to foster revitalization and investment in the historic Downtown core. The Town Center Specific Plan envisioned a community-oriented, walkable, and economically vibrant Downtown in the heart of Covina. This plan was envisioned to capitalize on the local history that has been preserved through the protection of historic buildings, as well as provide residents with the opportunity to experience a traditional city center with a small-town atmosphere. Much has been accomplished under the original Specific Plan; however, demographics and market changes dictate the need for a more flexible, adaptive, market-driven Specific Plan. While some progress has been made in the ensuing years, change has been slow and incremental. Simply put, Downtown Covina is still not reaching its full potential.

In 2015, the City was awarded a Metro TOD Grant to improve connectivity between the Metrolink Station and Downtown Covina. With this goal and funding as the initial catalyst, the objective of this new Covina Town Center

Specific Plan (CTCSP) is to revitalize the city core and streamline the development process while building upon the Covina Metrolink Station's assets.

The CTCSP is an action-oriented plan that outlines specific land use and mobility opportunities within the Town Center, as well as standards and guidelines for new buildings' form. The Specific Plan seeks to create a memorable, accessible, and economically vibrant Town Center. The Specific Plan is the result of extensive community engagement, planning, and design efforts. It builds upon community desires and aspirations from residents, businesses, property owners, stakeholders, City staff, the Planning Commission, and City Council.

The CTCSP aims to be truly transformative. It implements the Town Center Vision, outlines development standards and design guidelines to support the vision, and identifies action steps to achieve key objectives.



KEY CONCEPTS

The CTCSP's key components are intended to work together to improve the Town Center for the future, balancing the diverse needs and desires of the Covina community. It is necessary to look at the policies, guidelines, standards, and the supportive images and maps in concert with one another to realize the full nature of the plan. A summary of the key concepts is listed below.

Placemaking: CTCSP's urban design strategies and approaches emphasize the investment, creation, and renovation of public space to enhance residents' quality of life and wellbeing; to attract investment, tenants, and consumers to the area; and create a "sense of place" for Downtown Covina.

Catalyst Sites: Development, adaptive reuse, or redevelopment of specific Town Center sites can drive development and revitalization activity over the life of this Specific Plan. These catalyst sites provide opportunities for integrated developments that contribute to the area's vibrancy and economic wellbeing and will serve as indicators of the Town Center's positive change and continued improvement.

Gateways: Key access points into the Town Center can communicate more than just a geographic location. Gateways, monuments,

landscaping, and other visual elements contribute to the Town Center's image and identity.

Walkable Environment: Pedestrian facilities such as wide sidewalks, curb extensions, parklets, and streetscape improvements (e.g. benches, shade trees, wayfinding signage, public art) create a safe, comfortable, memorable, and connected pedestrian realm that reinforces the Town Center's sense of place.

Quality Design: High quality materials and strong placemaking character in landscape plantings, lighting, street furniture, crosswalks, and other streetscape components shape human perceptions of the area and serve to draw people into the Town Center. In addition, the buildings' exterior architectural features and placement on a site are critical in defining the building's relationship to the street, contributing to the built environment's character.

THE CTCSP PROVIDES A LONG PLAN PURPOSE

The CTCSP provides a long-term strategy for revitalizing Downtown Covina, encouraging transit-oriented development, and creating a more memorable, accessible, economically vibrant, and regionally attractive Downtown. Through concerted, strategic efforts, the Town Center has the potential to become an iconic and attractive place for new and existing businesses to flourish, accompanied by new residential developments, shopping opportunities, and innovative land uses and entertainment uses in an inviting, walkable, and bikeable environment.

The CTCSP provides the necessary framework to guide future public and private investment in the Town Center over a 15-year planning horizon. In general, development activity is stimulated and influenced by a range of tools, including:

- Development standards, design guidelines, and other regulatory tools and metrics
- Public infrastructure improvements
- Comprehensive and strategic set of policy, physical, and programmatic implementation actions

These elements are an impetus and guide tangible change to the Town Center. This document represents the community's cohesive vision and provides solutions to transform the area into a memorable, vibrant, pedestrian-oriented, interconnected, and attractive Town Center.

WHAT IS A SPECIFIC PLAN?

A specific plan is a tool to implement the City's General Plan. It establishes a link between General Plan policies and individual development proposals in a defined area. State law requires that specific plans be consistent with the local General Plan. The Covina Town Center Specific Plan (CTCSP) is a direct implementation measure of the Covina General Plan, which calls for creation of a Specific Plan in this area to foster mixed-use development and overall revitalization.



Pedestrians strolling along Citrus Avenue

PLAN CONTEXT

Area Context

Covina is located in Los Angeles County, California, at the southern base of the San Gabriel Mountains. It borders Baldwin Park and Irwindale to the west, Glendora and Azusa to the north, San Dimas and Pomona to the east, and West Covina to the south. Covina has a total area of approximately seven square miles.

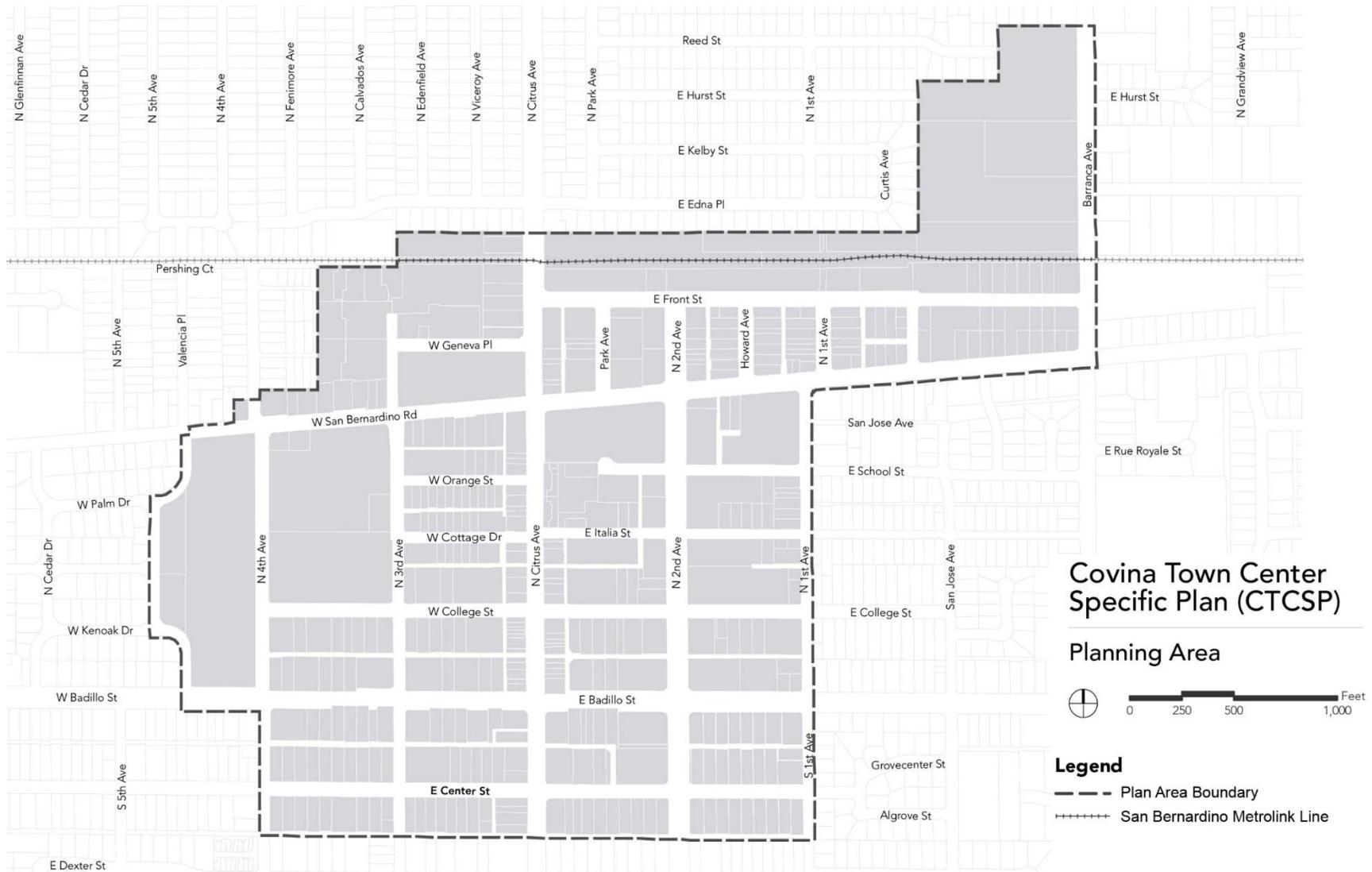
Covina is connected to neighboring cities and regionally by the Interstate 210 (I-210) and the Interstate 10 (I-10), and the Orange Freeway (State Route 57). Badillo Street also provides a link to civic and commercial uses within Covina, while key north-south corridors such as Citrus Avenue and Barranca Avenue connect retail services, medical-related uses, and business park areas to the city's residential neighborhoods. The Covina Metrolink Station, located on the northern edge of the historic Downtown, connects Covina to Los Angeles and San Bernardino via the San Bernardino Line.

Planning Boundary

The CTCSP and the Town Center is roughly bound on the north by the Metrolink/Union Pacific railroad tracks and Kelby Park, on the south by Center Street, on the west by 4th Avenue and Covina Park, and the east by North 1st Avenue and Barranca Avenue.

Figure 1-1: Planning Area

Figure 1-1

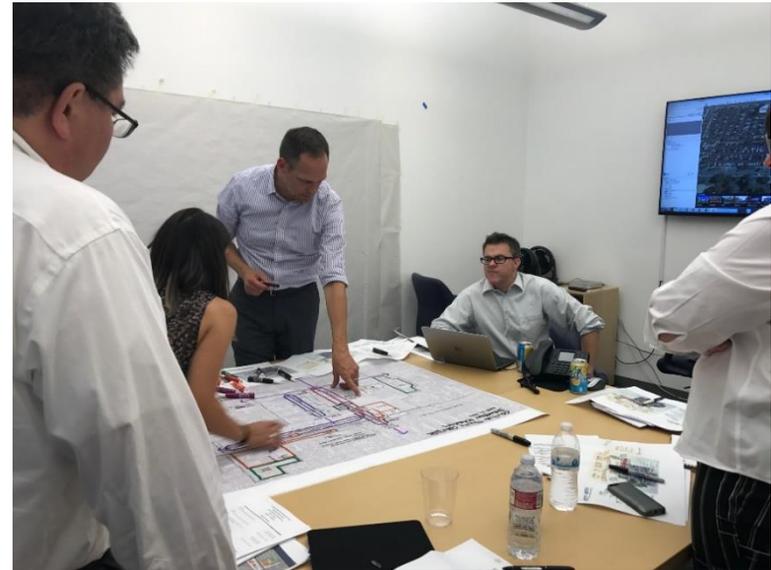


SPECIFIC PLAN PROCESS

The CTCSP is the culmination of a broad community-based process, beginning in the early 2000s with the Covina General Plan and continuing in 2015 with a Metro TOD Grant award driving the current revision of the CTCSP. Drawing from early visioning efforts as the primary foundation, the CTCSP future vision involved close coordination with local stakeholders, business and property owners, neighborhood representatives, elected and appointed officials, and members of the public representing a broad cross-section of Covina. A multi-disciplinary consultant team, led by MIG, Inc., supported the planning process and performed necessary technical analyses for the Specific Plan effort. The consultant team included urban planners, designers, economists, transportation planners, and environmental specialists.

COMMUNITY ENGAGEMENT

Early in the process, a series of stakeholder interviews were conducted to assess existing conditions, issues, and solutions. Meetings were held with stakeholders, City Council, and Planning Commission members to discuss issues and opportunities, develop the Specific Plan Framework, and review the Draft Specific Plan Concepts. Participants voiced aspirations for the future of the Covina Town Center; discussed the challenges, issues, and opportunities currently facing the area; and expressed ideas for enhancing the Town Center and better connecting it to the Covina Metrolink Station. The vision and overarching goals for the Covina Town Center, desired land uses and development framework, identified catalytic opportunity sites, and the standards and guidelines that form the basis of this Specific Plan have been driven by community feedback.



Input from community members and City representatives are foundational to development of the CTCSP.

Figure 1-2

HOW TO USE THIS PLAN

This Specific Plan is designed to be easily understood and referenced by a range of users, including City staff, elected officials, developers, business and property owners, and community members. Residents interested in the Covina Town Center’s future or ongoing development activity can focus on the broad policy concepts in the Plan (**Chapter 2: Vision, Framework, and Strategies**) or the more detailed information related to the type, location, and intensity of uses allowed in the CTCSP’s specific districts (**Chapter 3: Land Use and Zoning**). Elected officials or City staff stewarding development in the area can also focus on the regulatory framework for future development in **Chapter 4: Design Standards and Guidelines**, the physical improvements in the public realm contained in **Chapter 5: Mobility and Infrastructure**, and the funding sources, land use and project approval, and timelines contained in **Chapter 6: Implementation**.

For property owners and developers, the following detailed steps are a quick way to understand the different sections of the plan and assist with navigation of the main components of the document.

Step 1: Find out what district applies to your project.

Locate your project location on **Town Center Districts (Figure 2-1)** to determine which Town Center district applies.

Review the narrative description for that district to determine whether your proposed project complies with the CTCSP’s intent for that area. Descriptions for each district are found in **Chapter Two: Vision, Framework, and Strategies**.

Step 2: Review the Land Use Tables.

Use Chapter 3’s **Land Use Designations map (Figure 3-1)** to determine land use designation for the property in question, then look to **Table 3-1 Land Use Designations** and **Table 3-2 Allowable Land Uses** to determine if your project is allowed in a particular designation and at what density or intensity.

Step 3: Review the Development Standards and Guidelines Text.

If your proposed use is allowed, refer to the design standards and guidelines located in **Chapter 4: Design Standards and Guidelines**.

Step 4: Follow the Appropriate Application Process.

Use the City’s application process to have your project reviewed and approved. (**Chapter 6: Implementation**)



Citrus Walk in Downtown Covina

DOCUMENT OVERVIEW

The CTCSP integrates a set of bold strategies to achieve the vision of a vibrant, thriving, connected, and memorable Town Center. Each chapter of this comprehensive document is described in detail below.

Chapter 1: Introduction

Presents the CTCSP's purpose and provides background information to contextualize the reader.

Chapter 2: Vision, Framework, and Strategies

Presents the community-based vision and framework that guided the formulation of the CTCSP.

Chapter 3: Land Use and Zoning

Provides the regulatory framework for future development, renovations, and ongoing maintenance within the CTCSP planning area. The chapter identifies allowable land uses and illustrates development standards tailored to each part of the Town Center.

Chapter 4: Design Guidelines and Standards

Sets the physical guidelines to perpetuate excellence in design and quality of craftsmanship to enhance the Town Center environment in both the public and private realms.

Chapter 5: Mobility and Infrastructure

Proposes physical improvement to the public rights-of-way geared towards establishing a robust mobility network with enhancements to streets, bicycle, pedestrian, and transit facilities. Chapter 5 also assesses the condition of existing infrastructure (water, storm drainage, sanitary sewer, and power lines) in relation to these systems' abilities to meet the demands of projected new growth.

Chapter 6: Implementation

Outlines major actions necessary to implement the vision, strategies, and concepts identified in the CTCSP, including the City's development application process. It includes a detailed Implementation Action Plan with specific action steps, funding sources, and timelines to implement incremental change.

Chapter 7: Glossary

Chapter 7 lists important terms and definitions used throughout the CTCSP. For purposes of carrying out the CTCSP's intent, words, phrases, and terms should be deemed to have the meaning ascribed to them and shall be interpreted to have the standards and include the parts, elements, and the features set forth in this Specific Plan. When not inconsistent with the context, words used in the present tense include the future; words in the singular number include the plural; words in the plural number include the singular. "Shall" is mandatory, "may" is permissive.

ENVIRONMENTAL REVIEW

The City of Covina is a lead agency under CEQA; the City of Covina is the authority to certify the Covina Town Center Specific Plan Environmental Impact Report (CTCSP EIR), prior to approving the CTCSP. Public Resources Code, Section 21081.6 also requires public agencies adopt a monitoring program to ensure that mitigation measures are implemented.

City Council certified the CTCSP EIR and adopted the associated Mitigation Monitoring and Reporting Program (MMRP) on November 29, 2019, Resolution CC 2019-119. The City of Covina is required to confirm that any applicable mitigation measures have been implemented in accordance with the adopted MMRP.

In conjunction with one another, it is the intent of the CTCSP and CTCSP EIR that the need for project-specific environmental review in areas that have been analyzed by the EIR are either eliminated or reduced, subject to findings of consistency that there are 1) no significant changes in conditions, and 2) that the project complies with the CTCSP's requirements. Certain projects may require additional specific environmental review, as necessary, if they do not conform with the CTCSP. This could include targeted studies on one or more identified environmental concerns. The City will make these determinations and environmental review may be incorporated in the development approval process.

It is anticipated that the approval of subsequent applications/projects consistent with the CTCSP will not require the preparation of new environmental documents unless otherwise required pursuant to Public Resources Code Section 21166, as may be amended from time to time.



CITY HALL

City Hall

1911

1911

COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 2: VISION, FRAMEWORK, AND STRATEGIES





Chapter 2 Contents

- Vision
- Vision Elements
- Town Center Districts & Strategies
- Town Center Districts

VISION, FRAMEWORK, AND STRATEGIES

COVINA TOWN CENTER VISION

Broad input garnered through community meetings and stakeholder interviews underpins the Covina Town Center Specific Plan (CTCSP) Vision Statement. Residents, business owners, property owners, neighborhood representatives, City staff, elected officials, and community members all helped shape and refine the vision.

VISION

*Covina's Town Center is a **walkable and vibrant activity center** that preserves its historic look and character while allowing for and promoting more innovative and flexible design and uses. The Town Center is well-connected to the surrounding neighborhoods, businesses, and regional Metrolink transit services.*

*Covina's Citrus Avenue is a **pedestrian-friendly and inviting** street that is a main focus for the Town Center and Covina as a whole. It serves as a primary north/south arterial and a gateway connecting the Covina Town Center to the Metrolink station. Improved bicycle and pedestrian connections safely link Covina's Town Center to adjacent businesses and institutions, including the Emanate Health Inter-Community Hospital.*

*Covina's Town Center is an **attractive mixed-use center** providing residents with an environment where they can live, work, and play. It is economically diverse and strong. Local businesses are supported and preserved while attracting new services and industries that complement and enhance Covina's rich history and*

identity. Vacant and underutilized site redevelopment is encouraged and supported to help create this authentic space.

*Covina's Town Center has a **distinct character and creates a sense of place that attracts and supports the community**. Covina fosters creativity and flexibility to advance its economic future and improve its accessibility and interconnectivity.*

The Covina Town Center is a unique gathering spot where residents, employees, business owners, and visitors come together to live, work, socialize, and shop.

VISION ELEMENTS

The following Vision Elements guide and provide a framework to achieve the aspirational future described in the Covina Town Center Vision. The Vision Elements provide the foundation for the standards, guidelines, and policy direction outlined in the CTCSP's subsequent chapters, directing the character and design of future private development and public improvements. These **concepts and tools work together to transform the physical, economic, and social environment** of the Town Center in the years to come.

Vision Element 1: Authentic: An attractive, memorable, and welcoming place

- Preserve Town Center’s historic look while adding “fresh” appeal.
- Restore aging industrial buildings and adapt for a diversity of uses, including residential, commercial, and business mixed-use spaces.
- Enhance Citrus Avenue’s historic character in the Downtown core.
- Encourage urban design features that celebrate Covina’s rich history.

Vision Element 2: Well-Connected: An accessible, walkable community

- Connect the Covina Metrolink Station to the Town Center via a “gateway” concept encouraging transit riders to safely and pleasantly walk from the station to the Town Center.
- Connect adjacent uses to the Covina Metrolink Station.
- Capitalize on alley connections to increase accessibility and walkability between the Town Center and adjacent districts.
- Improve bike and pedestrian networks to increase safety and connectivity to the greater community.
- Add signage and wayfinding to increase navigability of the Downtown core.
- Enhance pedestrian connections between the hospital and Downtown core.

Vision Element 3: Economically Viable: A thriving, robust economy

- Create a Transit-Oriented Development (TOD) hub at the Covina Metrolink Station incorporating housing and creating a dynamic environment for residents and visitors.
- Capitalize on the hospital’s investments and facilitate growth of a medical core.
- Encourage business to expand their hours, thereby attracting commuters and local employees to patronize shops and restaurants after work.
- Expand the local market to focus on experiences, goods, and services not currently available in the Town Center nor online.

Vision Element 4: Flexible: Creative, adaptable policy and design

- Allow flexibility of uses to create new active spaces.
- Implement innovative parking strategies and better utilize existing parking.
- Allow heights and densities that are responsive to the market.
- Attract multifamily residential development.
- Develop a balanced “live/work/play” environment.

VISION VS. STRATEGY

The Vision for the Town Center is a forward-looking, aspirational description of the Town Center. It serves as a clear guide for choosing courses of action. The Strategy is a plan for accomplishing the Vision. The Strategy contains the course of action required to meet the end goal embodied in the Vision statement: a unique gathering spot where residents, employees, business owners, and visitors come together to live, work, socialize, and shop. While circumstances may change, the Vision should endure for many years to come. The Strategy can adapt to the changes in the marketplace or to the needs and desires of the City, its residents, and property owners.



TOWN CENTER DISTRICTS AND STRATEGIES

The CTCSP provides a long-term strategy to achieve the Town Center’s development potential; it is comprised of several tools that facilitate the development of a well-organized, clearly identifiable and economically viable Town Center. To facilitate revitalization strategies authentic to each sub-area, the Town Center is divided into five districts. Characteristics vary among the districts that reflect land use, building scale, architectural style, and other factors. The CTCSP land use approach emphasizes and enhances these differences. Key strategies are established for the following districts:

Front Street/TOD/Food, Arts, Industrial and Residential (F.A.I.R.) District

Citrus Avenue District

Downtown West District

Civic and Cultural Core District

Center Street Neighborhood District

The Front Street/TOD/F.A.I.R. District, north of San Bernardino Road, has long been an industrial area with small lot, light industrial uses. Citrus Avenue’s commercial and mixed-use buildings have historically dominated the district. Along and east of Citrus Avenue, the Covina Metrolink Station’s surface and structured parking dominate. Additional

public lots and/or structures are available throughout the Town Center including immediately behind the Citrus Ave facing buildings, and on College Street. Downtown West and the Civic and Cultural Core Districts have concentrations of medical and civic uses, respectively. The Center Street Neighborhood District is a residential area transitioning from single-family to multifamily residential uses. The **Land Use Zoning Designations (Figure 3-1)**, along with development standards and design guidelines, build upon the strengths of each district shown on **Figure 2-2. Chapter 4: Design Standards and Guidelines** provides a framework for the Town Center’s public improvements, site design, and building architecture.

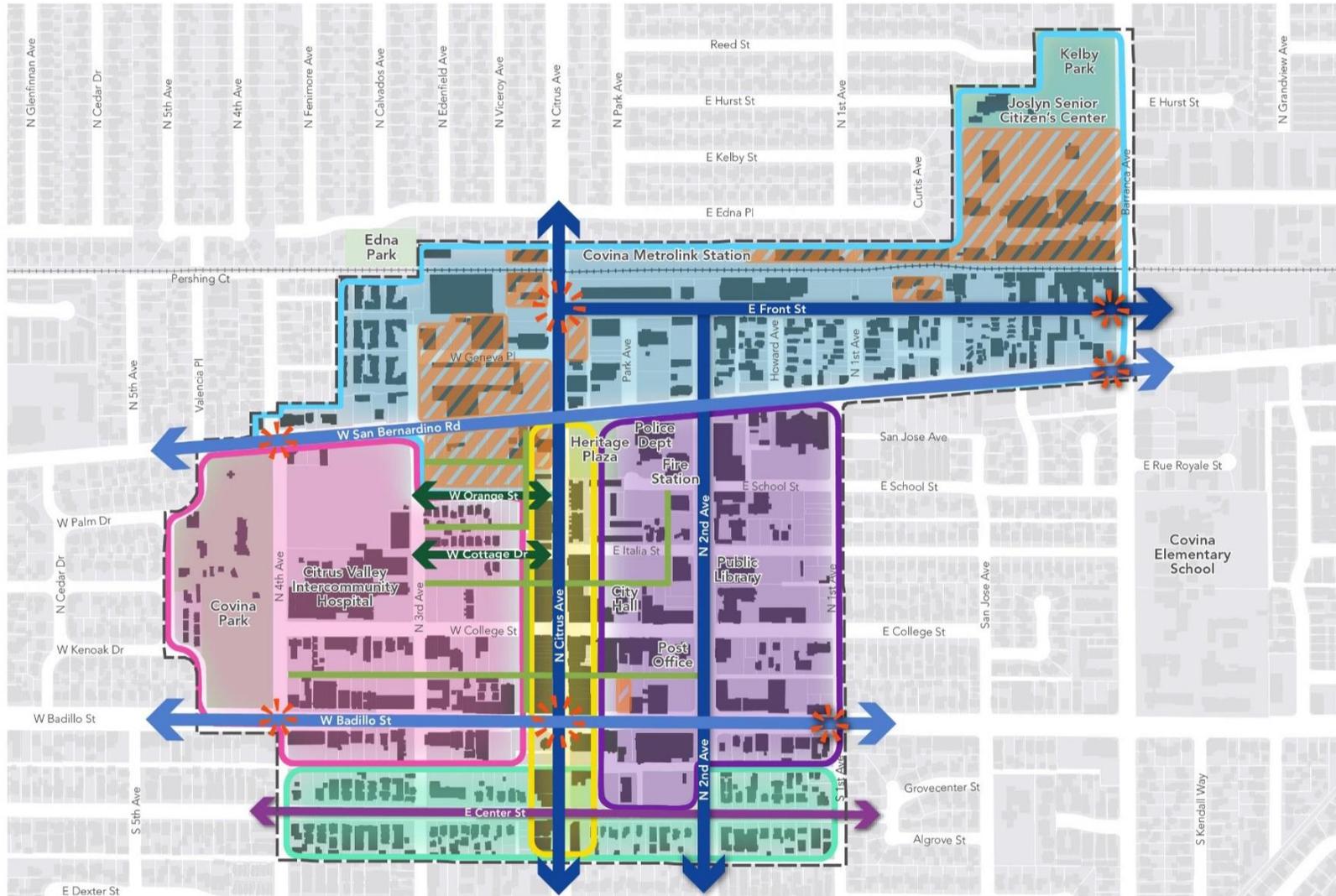
Key strategies are summarized below by district.

Catalyst Site – Public or private development site that is planned to cause a corresponding and complementary development reaction on surrounding properties or within the district.

Gateway – A key location at the edge of the Covina Town Center Specific Plan area where a placemaking entry monument should be placed noting the entrance into/exit from the Covina Town Center.

5 TOWN CENTER DISTRICTS (REVISED 9-20-2022)

Figure 2-1



Covina Town Center Specific Plan

Strategy



Legend

- Plan Area
- San Bernardino Metrolink Line
- Building
- Parcel

- Front Street / TOD
- Citrus Avenue
- Downtown West
- Civic and Cultural Core
- Center Street Neighborhood

- Regional Connector
- Local Connector
- Health Corridor
- Residential Street
- Key Alley

- Catalytic Site
- Gateway

TOWN CENTER DISTRICTS REVISED | Front Street/TOD/F.A.I.R. District (food, arts, industrial, and residential)
 (REVISED 9-20-2022)

Figure 2-2



Front Street/TOD/F.A.I.R. District (Revised 9-20-2022)

The Front Street/TOD/Food, Arts, Industrial and Residential (F.A.I.R.) District is the northernmost Town Center district. This area is projected to **transform from mostly industrial land uses into a combination high-density housing and a “maker’s district” or “arts district,”** with creative uses such as artist studios and galleries, live/work units, restaurants, small-scale craft breweries, and creative office activities. With many catalyst site opportunities, new residential and commercial/industrial uses will leverage proximity to the Covina Metrolink Station and create better connections to the Town Center’s historic core. Catalyst sites or other sites in this F.A.I.R. District that are within ½ (one-half) mile of Covina Metrolink station may request to use the Transit Oriented Development /High Density Residential (TOD/HDR) Zone for land use density, design guidelines and standards for new mixed-use development or standalone residential project, subject to a Conditional Use Permit. Creative approaches to parking and public spaces along Front Street, as well as the creation of comfortable pedestrian environments, are vital to transforming this district.

Placemaking Sites: Covina Metrolink Station and supportive parking, Kelby Park

General Boundaries:

- North - Kelby Park and the Covina Metrolink Station
- South – San Bernardino Road and West Orange Street
- East – Barranca Avenue
- West – Palland Lane and North 4th Avenue

Character: Transit-oriented, maker’s district, industrial

Catalyst Sites:

- Vita Pakt
- Front Street/Curtis Avenue
- Citrus Avenue/Rail adjacent
- Front Street/Citrus Avenue
- West Geneva Place/3rd Avenue
- San Bernardino Road/Citrus Avenue/3rd Avenue
- West Orange Street/Citrus Avenue/3rd Avenue

Gateways:

- East Front Street/North Citrus Avenue – primary
- East Front Street/Barranca Avenue – secondary
- East San Bernardino Road/Barranca Avenue – secondary
- West San Bernardino Road/North 4th Avenue – secondary

FRONT STREET/TOD/F.A.I.R. BIG IDEAS

- “Maker’s District” including live/work development
- Front Street lined with landscaping and pedestrian amenities
- Pedestrian bridge crossing over railroad

Figure 2-3

Front Street/TOD/F.A.I.R. District



INTERIM IMPROVEMENTS

- 1 Temporary Landscape Buffers
- 2 Midblock Crossing
- 3 Bike Sharrows

FUTURE IMPROVEMENTS

- 4 Plaza
- 5 ADA Compliant Curb Cut
- 6 Permanent Landscaping
- 7 Multi-modal Lanes



TOWN CENTER DISTRICTS | Citrus Avenue District *Figure 2-4*



Covina Town Center Specific Plan (CTCSP)

Citrus Avenue

⊕ Illustrative Map not to scale

Legend

- Plan Area
- San Bernardino Metrolink Line
- Building
- Parcel
- Citrus Avenue Area
- Citrus Avenue Boundary
- ⊙ Gateway
- ↔ Regional Connector
- ↔ Local Connector
- ↔ Health Corridor
- ↔ Residential Street
- Key Alley

Citrus Avenue District

Citrus Avenue District, Covina’s historic downtown and central core, links all CTCSP Districts to each other and to the Covina Metrolink Station. The Citrus Avenue District’s strategy prioritizes **historic character preservation and enhancement while addressing commercial uses’ underutilization through attraction of visitor-friendly and sensitively-designed retail, services, and mixed-uses.**

Envisioned to be vibrant, strong, and connected with a strong sense of place, an integrated and well-designed public realm is critical to achieving the Citrus Avenue District vision. Safe, accessible, and well-placed sidewalks and crosswalks are essential for an activated Town Center setting, allowing people to “park once” and easily visit area businesses and institutions on foot. Street furniture invites people to linger outside and encourages social activity by making the public realm more comfortable and convenient. Parklets repurpose small segments of streets into public spaces. These small parks, generally located in a single on-street parking space, provide amenities like seating, planting, bicycle parking, and art. Plazas help to enliven commercial centers and add visual interest. Intimately scaled outdoor areas and plazas accommodate daily activities such as eating and relaxing. They can also create space for public events such as ceremonies, festivals, and live performances. Plazas should incorporate shade and rest areas, as well as drinking fountains, water features, trash cans, accent lighting, public art, and other similar enhancements that encourage use and social gathering. Parklets and plazas help encourage social activity by creating community spaces where people can linger, dine, or simply “people-watch”, creating a more exciting pedestrian realm.

Citrus Avenue District’s public open space should revolve around Heritage Plaza’s activation, which provides a unique opportunity in the focal point of the Town Center. Frequent, programmed, day and night use – mobile library kiosks, art/recreation classes, concerts/movies – will bring people to the Town Center and provide a place for social interaction and people-watching. The District’s open space will extend through a series of Citrus Avenue parklets and landscaped bulb-outs.

Public parking lots, located behind the Citrus Avenue buildings, must be refreshed and greened, to improve environmental quality while they continue to support Citrus Avenue’s limited on-street parking. The Citrus Avenue District continues south of Badillo Street to provide pedestrian connections to the Center Street Neighborhood district and beyond.

Placemaking Sites: Heritage Plaza, Alleys

General Boundaries:

- North – Heritage Plaza
- South – East and West Center Street
- East – Citrus Avenue facing parcels and alleys immediately east of Citrus Avenue
- West – Citrus Avenue facing parcels and alleys immediately west of Citrus Avenue

Character: Historic, “strollable” downtown

Gateway: Badillo Street/Citrus Avenue – primary

CITRUS AVENUE BIG IDEAS

- “Park once” concept
- Parklets
- Plazas
- Landscaped bulb-outs

Figure 2-5

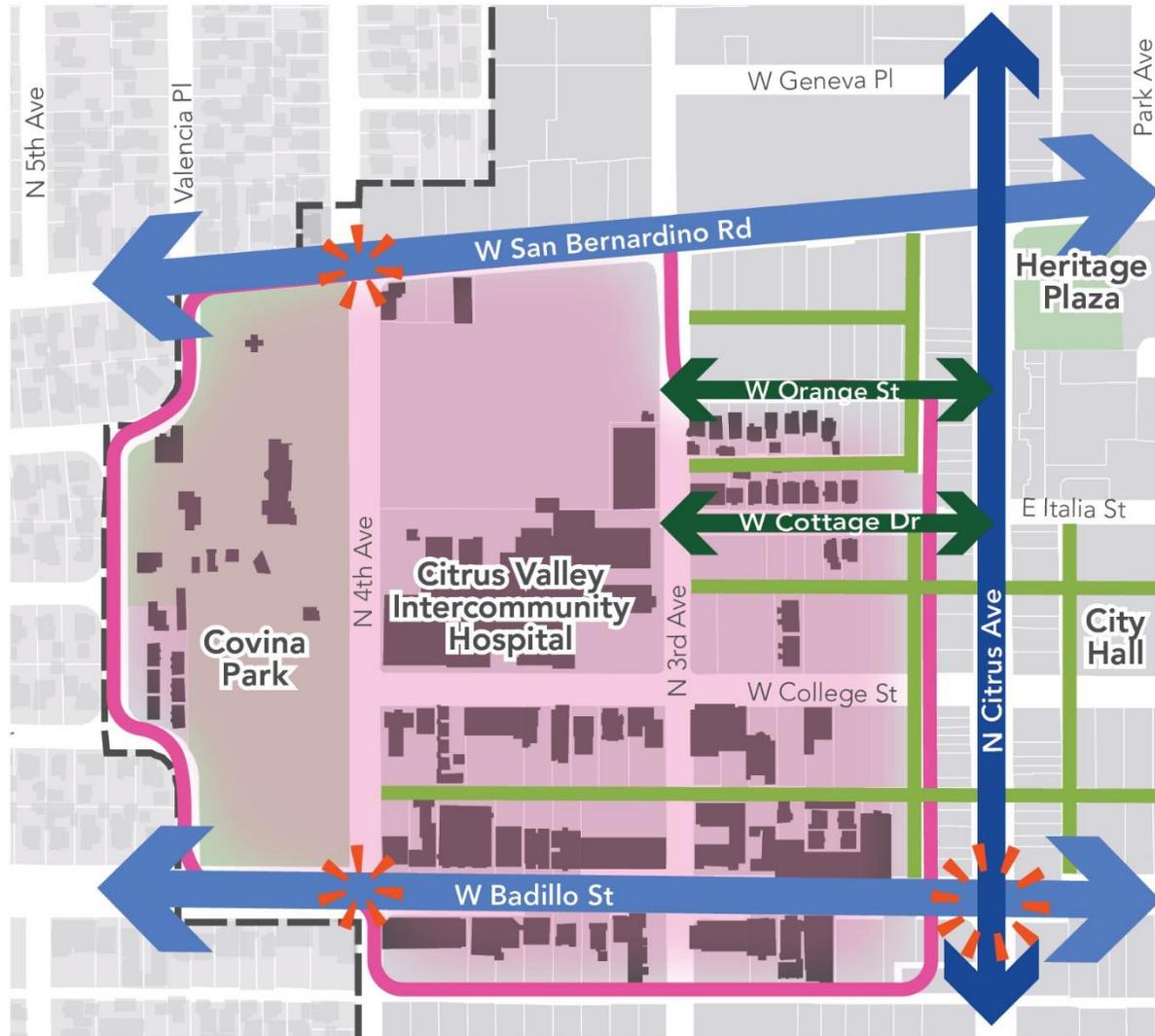
Citrus Ave



FUTURE IMPROVEMENTS

- 1 On-Street Dining
- 2 Enhanced Paving
- 3 Enhanced Landscaping

Figure 2-6



Covina Town Center Specific Plan (CTCSP)

Downtown West

- Illustrative map not to scale
- Legend**
- Plan Area
- San Bernardino Metrolink Line
- Building
- Parcel
- Downtown West Boundary
- Downtown West Area
- Gateway
- Regional Connector
- Local Connector
- Health Corridor
- Key Alley

Downtown West District

Downtown West District is centrally located immediately west of the Citrus Avenue District between Front Street/TOD/F.A.I.R. and Center Street Neighborhood Districts. The Downtown West District's strategy aims to **focus, concentrate, and enhance its medical and support uses**, thus creating a distinct, active medical district.

Creative pedestrian connections or "health corridors" between the Emanate Health Inter-Community Hospital/Covina Park and Citrus Avenue will enhance connectivity and improve visitor attraction and retention for the Town Center's commercial areas. Adapting North 3rd Avenue (between West College and West Cottage) into a pedestrian plaza would serve as an entrance to the Emanate Health Inter-Community Hospital and encourage pedestrian activity between the medical uses and Citrus Avenue uses. The adaptive reuse of West Orange and West Cottage single family dwellings to office or live/work uses and the preservation of the historic aesthetic is encouraged. Constructing parking structures and establishing shared parking programs between the medical offices, hospital, and Covina Park will free up surface parking lots for medical services or hospital expansion/redevelopment and open space.

Placemaking Sites: Emanate Health Inter-Community Hospital, Covina Heritage House, Covina Park, North 3rd Avenue

General Boundaries:

- North – West San Bernardino Road
- South – West Badillo Street
- East – West 3rd Avenue, alley immediately west of Citrus Avenue
- West – North Valencia Place

Character: Urban medical campus

Catalyst Sites: Inter-Community Surface Parking Lots

Gateways:

- West San Bernardino Road/North 4th Avenue – secondary
- West Badillo Street/North 4th Avenue – secondary

DOWNTOWN WEST BIG IDEAS

- Health corridors linking hospital to Citrus Avenue
- North 3rd Avenue Pedestrian Plaza/Hospital entrance
- Adaptive reuse of residential structures
- Parking structures replacing surface lots – freeing up space for medical use expansion

Figure 2-7

Downtown West



FUTURE IMPROVEMENTS

- 1 Enhanced Landscapes
- 2 Multimodal Streets
- 3 Nature Garden/bio swales
- 4 Enhanced sidewalks

See Chapter 5 for the bicycle and enhanced sidewalk details.

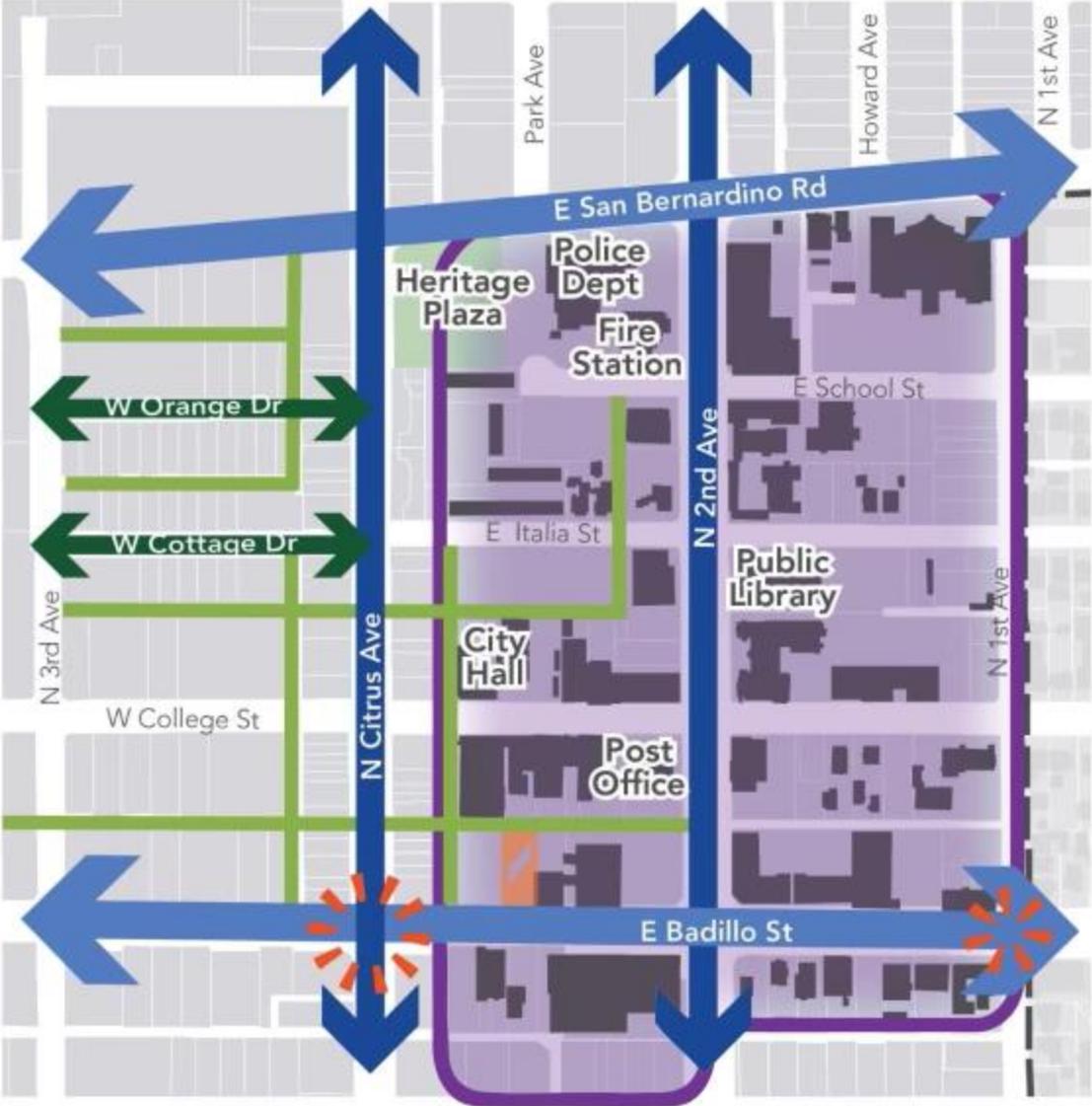


Figure 2-8

Covina Town Center Specific Plan (CTCSP)

Civic and Cultural Core

⊕ Illustrative Map not to scale

Legend

- Plan Area
- San Bernardino Metrolink Line
- Building
- Parcel
- Civic and Cultural Core Area
- ▬ Civic and Cultural Core Boundary
- ▨ Catalytic Site
- ★ Gateway
- ↔ Regional Connector
- ↔ Local Connector
- ↔ Health Corridor
- ↔ Residential Street
- Key Alley

Civic and Cultural Core District (Revised 9-20-2022)

The Civic and Cultural Core District is a critical component of creating a true “Town Center”. The strategy for this district is to **leverage and build upon its many government, cultural, and arts-related assets** including the Police Station, Fire Station, Post Office, Covina Public Library, City Hall and Heritage Plaza, as well as religious institutions and art museums. Expanding these facilities beyond their walls is crucial to creating a vibrant Town Center.

Several strategies are important to further activating the area. Re-establishing the library’s entrance on North 2nd Avenue and creating an outdoor library plaza with mobile library carts, outdoor furniture, and coffee/drink kiosks will help activate 2nd Avenue. Creating a landscaped plaza along the Italia Street parking lot’s southern edge will “green” the parking lot and provide a pleasant pedestrian paseo between 2nd and Citrus avenues. The Italia Street plaza encourages College Street businesses and governmental uses to establish outdoor dining and active spaces along the landscaped plaza, including an expanded City Hall demonstration garden. Installing a 2nd Avenue landscaped median and extending the roadway’s sidewalks will help supplement the recently-painted bike lanes to transform the wide street into a comfortable, walkable pedestrian/bike-oriented connection through the Civic and Cultural Core. The many public parking facilities – both surface and structured – provide a strong foundation to create a shared parking program that would allow nearby private uses (religious institutions, shops) to transform their infrequently-used parking lots into usable visitor-serving space.

Redevelopment of vacant and underutilized site is encouraged and supported. Vacant or underutilized sites within ½ (one-half) mile of Covina Metrolink station may request to use the Transit Oriented Development /High Density Residential (TOD/HDR) Zone for land use density, design guidelines and standards for new mixed-use development or standalone residential development, subject to a Conditional Use Permit.

CIVIC AND CULTURAL CORE BIG IDEAS

- 2nd Avenue library entrance
- 2nd Avenue enhanced walkability and landscaping
- Library plaza
- Italia Street pedestrian paseos and outdoor use
- Shared parking program

Placemaking Sites: Covina City Hall, Public Library, Fire and Police Stations, Heritage Park

General Boundaries:

- North – East San Bernardino Road
- South – Parcels immediately south of East Badillo Street
- East – North 1st Avenue
- West – Alley immediately east of Citrus Avenue

Character: Visitor-serving civic and cultural town center

Catalyst Site: Mid-block East Badillo Street

Gateway: East Badillo Street/1st Avenue - secondary

Figure 2-9

Civic and Cultural Core



INTERIM IMPROVEMENTS

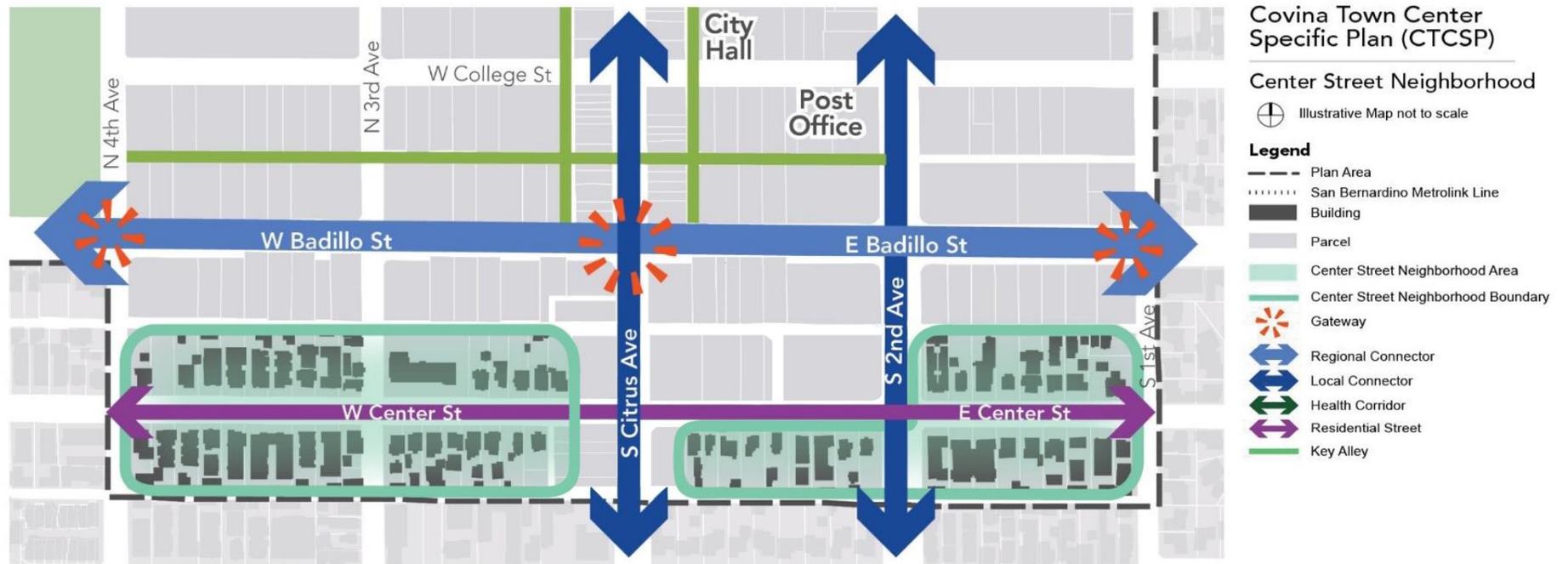
- 1 Temporary Enhanced Landscaping
- 2 Enhanced Crosswalks
- 3 Sharrow

FUTURE IMPROVEMENTS

- 4 Landscaped Median
- 5 Bulbout
- 6 Cycle Track
- 7 Library Plaza



Figure 2-10



Center Street Neighborhood District

The Center Street Neighborhood District, the area's southern-most district, spans from South 1st Avenue to South 4th Avenue and straddles Citrus Avenue. The Center Street Neighborhood District is distinct from all other districts in that it is a true residential neighborhood with older single-family homes and newer multifamily homes. This District's strategy focuses on the **preservation of the single-family residential uses and their traditional character, while providing design standards/guidelines for new multifamily uses** to develop in its two western-most blocks.

General Boundaries:

- North – Alley between Badillo Street and Center Street South – Alley between Center Street and Dexter Street
- East – South 1st Avenue
- West – South 4th Avenue

Character: Residential neighborhood

CENTER STREET NEIGHBORHOOD BIG IDEAS

- Single-family units' character preservation
- Multifamily use standards and guidelines

COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 3: LAND USE AND ZONING





A.I.R. DISTRICT

HARVEYS CALIFORNIA

Law Offices

Chapter 3 Contents

- Land Use Designations
- Area Guidelines and Standards
- Allowable Land Use by Zoning Designation

LAND USE AND ZONING

INTRODUCTION

Land use and zoning form the regulatory foundation to support and advance the Covina Town Center improvements. The **Land Use and Zoning chapter describes the type, location, and intensity of specific land uses allowed** within the Covina Town Center. Specific Plan land use designations are established to regulate allowed uses in the Covina Town Center and transform the area into a bustling district that has a supportive relationship with the Metrolink station, leading to increased passenger ridership. Building on the use regulations presented here, the development standards and guidelines (in **Chapter 4**) tailor and shape building form and the public realm.

The use and development standards of this chapter **apply to all subdivisions, new land uses, new structures, and substantial rehabilitation, alterations, and/or remodeling of existing land uses and structures.** Each of these shall be designed, constructed, and established in compliance with this Chapter. General maintenance and repair or other minor construction activities that do not result in an intensification of the use will not be regulated by this Plan but may be subject to other City permits and approvals prior to commencement.

Where a provision contained in this Chapter does not discuss a specific condition or situation that arises, the provisions set forth in the Covina Municipal Code (CMC) shall apply. In the event of a **conflict between these provisions and the CMC, the provisions set forth in the Covina Town Center Specific Plan shall govern.** In addition to these standards, all uses within the Covina Town Center Specific Plan (CTCSP) boundaries shall comply with all applicable local, state, and federal laws, ordinances, and regulations.

TOWN CENTER DISTRICTS AND STRATEGIES (CHAPTER 2)

To facilitate revitalization strategies authentic to each sub-area, the Town Center is divided into five districts (Figure 2-2):

- Front Street/TOD/Food, Arts, Industrial and Residential (F.A.I.R.) District
- Citrus Avenue District
- Downtown West District
- Civic and Cultural Core District
- Center Street Neighborhood District

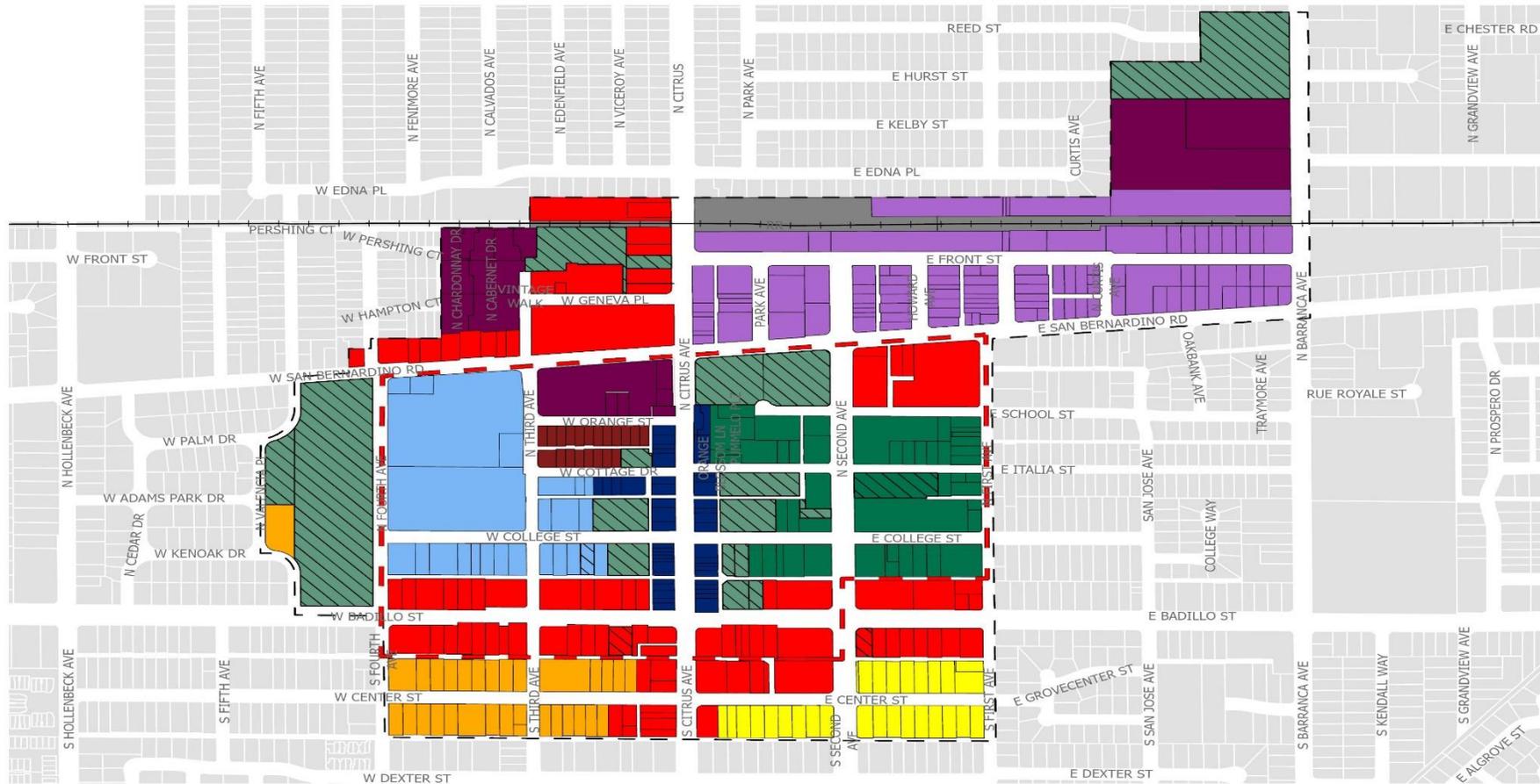
THE LAND USE AND ZONING (CHAPTER 3)

The Eleven Zoning Designations (Figure 3-1) and the Allowable Land Uses (Table 3-2) for each Zoning Designation, along with the development standards and design guidelines, build upon the strengths of each district shown on Figure 2-2.

11 ZONING DESIGNATIONS (Revised 9-20-22; 4-18-23)

Figure 3-1

The CTCSP zoning designations regulate the allowable uses within the Plan area (Figure 3-1 and Table 3-1). The descriptions for each land use designation below reflect future conditions



Covina Town Center Specific Plan

Planning Area



Legend

- Downtown Core Area Prohibiting Cannabis Businesses
- Plan Area Boundary
- City Owned Property
- San Bernardino Metrolink Line

Source: MG and City of Covina, 2023

Zoning

- Residential Town Center East
- Residential Town Center West
- Neighborhood
- TOD/HDR

- Mixed Use
- F.A.I.R.
- Historical Core
- Civic
- Cultural Core

- Medical Core
- Rail

TABLE 3-1: Density and Intensity (FAR)

ZONING DESIGNATIONS (See Figure 3-1)	Residential Density (units/acre)		Non-Residential Building Intensity (FAR)	
	Parcel Size		Parcel Size	
	Less than 0.5 acre	0.51 acre or more	Less than 0.5 acre	0.51 acre or more
Residential Town Center West	7 - 14	14 - 22	--	--
Residential Town Center East	7 - 14	14 - 22	--	--
Neighborhood	10 - 14	14 - 22	0.25	0.35
Transit Oriented Development/High Density Residential (TOD/HDR)	Stand-Alone Residential: 20 - 25 Mixed Use: 26 - 45	Stand-Alone Residential: 26 - 40 Mixed Use: 26 - 50	Stand-Alone Nonresidential: 1.0 Mixed Use: 2.5	Stand-Alone Nonresidential: 2.0 Mixed Use: 3.0
Mixed-Use	Stand-Alone Residential: 14 - 22 Mixed Use: 22 - 25	Stand-Alone Residential: 23 - 31 Mixed Use: 26 - 40	Stand-Alone Residential: 0.75 Mixed Use: 1.5	Stand-Alone Residential: 1.0 Mixed Use: 2.0
Food Arts Industrial Residential (F.A.I.R.)	Stand-Alone Residential: 10 - 14 Mixed Use: 1 - 15	Stand-Alone Residential: 15 - 25 Mixed Use: 1 - 25	Stand-Alone Industrial: 0.5 Stand-Alone Commercial: 0.75 Mixed Use: 1.5	Stand-Alone Industrial: 0.75 Stand-Alone Commercial: 1.0 Mixed Use: 2.0

Revised 9-1-2020 - Resolution CC 2020-80
Revised 9-20-2022 – Resolution CC 2022-99

CONT'D TABLE 3-1: Density and Intensity

ZONING DESIGNATIONS (See Figure 3-1)	Residential Density (units/acre)		Non-Residential Building Intensity (FAR)	
	Parcel Size		Parcel Size	
	Less than 0.5 acre	0.51 acre or more	Less than 0.5 acre	0.51 acre or more
Historic Core	Stand- Alone Residential: 15 – 20 Mixed Use: 20 - 25	Stand- Alone Residential: 15 – 30 Mixed Use: 26 - 40	Stand-Alone: 1.5 Mixed Use: 2.0	Stand-Alone: 2.0 Mixed Use: 2.5
Civic	Civic (including government office)/Residential Mixed Use: 20 - 25	Civic (including government office)/ Residential Mixed Use: 31 - 40	Stand Alone Residential: 0.5 Civic (including governmental office)/ Residential Mixed Use: 0.75 Commercial or Institutional: 0.75 Civic (including governmental office)/Commercial Mixed Use: 1.5 Government Office: 2.0	Stand Alone Residential: 0.75 Civic (including governmental office)/ Residential Mixed Use: 1.0 Commercial or Institutional: 1.0 Civic (including governmental office)/Commercial Mixed Use: 1.75 Government Office: 2.5

ZONING DESIGNATIONS (See Figure 3-1)	Residential Density (units/acre)		Non-Residential Building Intensity (FAR)	
	Parcel Size		Parcel Size	
	Less than 0.5 acre	0.51 acre or more	Less than 0.5 acre	0.51 acre or more
Cultural Core	Stand- Alone Residential: 10 – 14	Stand- Alone Residential: 15 – 30	Commercial or Institutional: 0.75 Government Office: 2.0	Commercial or Institutional: 1.0 Government Office: 2.5
Medical Core	NA	NA	Congregate care, Supporting Medical/ Commercial: 0.5 Medical Office: 1.5 Hospital: 3.0	Congregate care, Supporting Medical/ Commercial: 0.75 Medical Office: 2.0 Hospital: 3.0
Rail	NA	NA	NA	NA

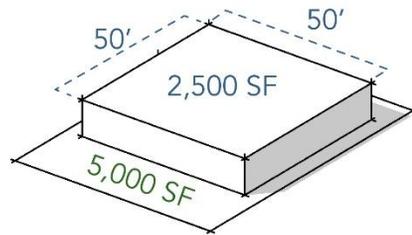
Calculating Floor Area Ratio (FAR): Density and Intensity

Each land use designation includes a measurement of development intensity. The measurement indicates the level of development that is permitted on each parcel within a given designation. For residential designations, this measurement is expressed as density – the number of units allowed per gross acre. For nonresidential uses such as institutional, commercial, or industrial, intensity is expressed as Floor Area Ratio (FAR). FAR is defined as the ratio between lot area and building floor area. For example, a 20,000 square foot parcel with a maximum allowable FAR of 2.0 would be permitted to have a 40,000 square foot building—example: Figure 3.2. In most cases, the land use

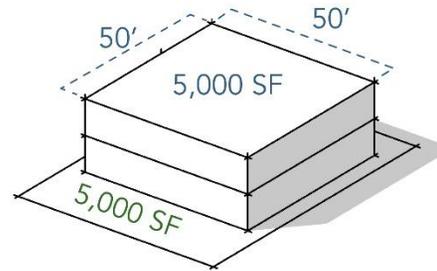
definitions only specify the maximum density or FAR that is permitted in an area. However, in a few cases, the definitions also specify the minimum intensity that may be required. This is particularly important for land around transit stations. Since the Covina Town Center has a limited supply of vacant or underutilized properties, it is important to use them efficiently. For example, it would be inconsistent to develop a property across the street from a large transit station with single family homes when such sites offer the best opportunities to reduce motor vehicle dependence, encourage transit ridership, provide higher density housing, and create pedestrian-oriented workplaces.

Figure 3-2

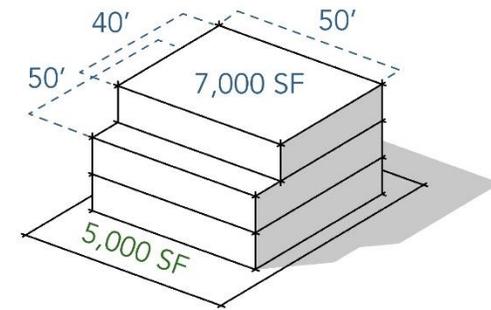
$$\text{FAR} = \frac{\text{Total Floor Area of Building}}{\text{Lot Area}}$$



$$\frac{\text{Building SF} = 2,500 \text{ SF}}{\text{Lot Area} = 5,000 \text{ SF}} = .5 \text{ FAR}$$



$$\frac{\text{Building SF} = 2,500 \times 2 \text{ SF}}{\text{Lot Area} = 5,000 \text{ SF}} = 1.0 \text{ FAR}$$



$$\begin{aligned} &\text{First and Second Floor} \\ &2,500 \times 2 = 5,000 \text{ SF} \end{aligned}$$

$$\begin{aligned} &\text{Third Floor} \\ &5,000 \text{ SF} + 2,000 \text{ SF} = 7,000 \text{ SF} \end{aligned}$$

$$\frac{\text{Building SF} = 7,000 \text{ SF}}{\text{Lot Area} = 5,000 \text{ SF}} = 1.4 \text{ FAR}$$

Area Guidelines and Standards

Residential Town Center West Zone

The **Residential Town Center West Zone** is made up of a compatible mix of single-family and medium-density homes near the City's historic core, in addition to hospital-related uses. Allowable residential formats include apartments, condominiums, townhomes, and single-family. Preservation of homes with historical residential architecture is prioritized and new development celebrates and complements the area's character. Specific Plan development standards (including Table 3-1) guide the ongoing transition of existing single-family uses to medium-density multifamily uses.

Residential Town Center West areas are characterized by older single-family homes and newer multifamily units. New development should exhibit attention to detail, quality architecture, and upscale materials. New multifamily units should complement the single-family character of the area.



Prominent Architectural Design and Materials of the Residential Town Center West Zone

- 1 Well-articulated façades
- 2 Front porch/eyes on street
- 3 Defined entry
- 4 Abundant private space landscaping
- 5 Modulated façade
- 6 Roof height variation
- 7 Multiple window/door openings



Figure 3-3

Residential Town Center East Zone

The area encompassing the **Residential Town Center East Zone** is a single-family residential neighborhood with historical architectural character and newer medium-density multifamily residential uses. Preservation of single-family homes with historical architecture is prioritized. The transition to medium-density multifamily uses with compatible character and scale is allowed.

Preservation of single-family residential is prioritized. New medium-density multifamily uses should have compatible character and scale.

See Table 3-1 for permitted development densities.



Figure 3-4

Prominent Architectural Design and Materials of the Residential Town Center East Zone

- 1 Varied materials to emphasize windows and doors
- 2 Façade color variation
- 3 Colors and materials act as design features
- 4 Modulated height
- 5 Defined entry

The **Neighborhood Zone** is home to a unique mix of single-family residential and small-scale office uses in structures with strong historical architectural character. Preservation of the historic character of the neighborhood and adaptive reuse of existing structures for small-scale office and medical uses is encouraged. Centrally located, new uses will complement and leverage the proximity to the Historic Core and TOD/HDR Zones, Emanate Health Inter-Community Hospital, and the nearby medical-related uses.

Preservation of the historical character of uses in the Neighborhood Zone and adaptive reuse of existing structures for small-scale office, live-work, or medical uses is encouraged.

See Table 3-1 for permitted intensities and densities.

Prominent Architectural Design and Materials of the Neighborhood Zone

- 1 Effective use of “front porch” space
- 2 Front porch/eyes on street
- 3 Distinctive entry walk
- 4 Varied building colors and materials
- 5 Roof height variation
- 6 Façade modulation
- 7 Distinct entry
- 8 Signage



Figure 3-5

Transit Oriented Development/High Density Residential Zone

The **Transit Oriented Development/High Density Residential (TOD/HDR) Zone** is a pedestrian-oriented, higher-density residential environment that supports and encourages transit use. New housing in varying forms and price ranges provides economic benefits to the area's residents. Commercial and service uses provide resources to transit riders and to the immediate and neighboring residential areas. High quality design features celebrate Covina's rich history and create a dynamic environment for residents and visitors. **Public open spaces are an integral component of large mixed-use or commercial developments.** The nonresidential uses provide services for residents that are not available in the Covina Town Center's historic and commercial cores. **Pedestrian connectivity between adjacent land uses is emphasized.**

New development should feature finely detailed and articulated building facades to create a rich character and scale. Open space amenities are important due to the lack of public space in the Covina Town Center's northern area.

See Table 3-1 for permitted intensities and densities.



Prominent Architectural Design and Materials of the Transit-Oriented Development/High Density Residential Zone



Figure 3-6

- 1 Well-articulated façades
- 2 Pleasant outdoor space, high level of landscaping
- 3 Varied building heights and colors
- 4 Articulated entrances
- 5 Private outdoor space
- 6 Pedestrian-oriented design at ground level
- 7 Multiple window openings

Mixed-Use Zone

The **Mixed-Use Zone** is an attractive mixed-use area that provides residents with an environment where they can live, work, and play. The zone includes a diverse mix of commercial, office, and residential uses that support and encourage pedestrian activity and provides easy access to civic, cultural, and medical resources. A walkable urban form – with prominent storefront/commercial activity and pedestrian connections along the Citrus Avenue, Badillo Street, and San Bernardino Road corridors – provides a safe and enjoyable experience for visitors and residents. Gateway concepts invite people to walk into the Covina Town Center core and to the Covina Metrolink station. Mixed uses are economically diverse and are differentiated from existing uses in the Covina Town Center’s historic core. Local businesses are supported and preserved while new services and industries complement and enhance Covina’s rich history and identity.

Development approaches encourage shared use of parking areas and public open spaces, pedestrian connectivity, and interaction of uses. Mixing of uses on properties (horizontal mixed use), within buildings (vertical mixed use), and within blocks - as compatibility allows - is emphasized though stand-alone uses, which are allowed at lower densities/intensities. Commercial ground-floor uses are required along Citrus Avenue. Uses near neighboring residential areas are developed at an appropriate scale and intensity to ensure compatibility. Redevelopment of vacant and underutilized sites is encouraged and supported.

Mixed-use developments should encourage pedestrian activity and be consistent in style with the established historical character of buildings in the Covina Town Center. Stand-alone uses are allowed at lower densities/intensities.

See Table 3-1 for permitted densities and intensities.

DESIGN STANDARDS & GUIDELINES

Design standards and guidelines, as well as detailed examples of building facade and architectural design for different land uses can be found in Chapter 4: Design Standards and Guidelines. Chapter 4 provides a framework for the Town Center’s private realm improvements, site design, and building architecture.

Mixed-Use Zone

Prominent Architectural Design and Materials of the Mixed-Use Zone

- 1 Façade modulation
- 2 Defined entries
- 3 Color variation
- 4 Multiple window/door openings
- 5 Compatible signage



Figure 3-7

Food Arts Industrial Residential (F.A.I.R.) Zone

The **Food Arts Industrial Residential (F.A.I.R.) Zone** is a transitional, “maker’s district” combining light industrial uses with creative uses such as artist studios and galleries, live/work units, restaurants, small-scale craft breweries, warehouses, incubator industrial uses, research and technology uses, and creative office activities. Stand-alone multifamily residential uses are permitted in new and adaptive reuse structures. Adaptive reuse of existing structures and the establishment of **shared parking, open spaces for recreation and public gathering** are prioritized. New **eclectic land use types with expanded business hours** capitalize on proximity to a regional transit station and a variety of civic and cultural destinations. Expansion of unique, non-industrial uses brings in goods and/or services not available in other parts of the Covina Town Center. Strong pedestrian connections to the Covina Metrolink station, nearby higher-density residential uses, and the historic core contribute to a vibrant activity center and destination for visitors, transit riders, and employees. Gateway concepts identify the neighborhood and invite people into the area.

Design approaches build on the industrial character of the neighborhood and flexible development standards accommodate and encourage the conversion of industrial facilities into creative and arts-related uses. **Reduced development requirements and encouragement of shared use of parking facilities** support the transition of land uses.

Adaptive reuse of existing structures in the F.A.I.R. Zone and the establishment of creative uses and public gathering places are desired. Uses that attract/retain transit riders and visitors are encouraged.

See Table 3-1 for permitted densities and intensities. Further, adaptive reuse or conversion of nonresidential structures into a residential use requires a minimum unit size of 450 square feet with all units averaging 750 square feet. Adaptive reuse must meet all building code requirements, while parking and open space requirements may be met offsite or with an in-lieu fee with the approval of the Community Development Director/designee. New structures may be constructed on a site where an adaptively reused structure exists, provided the overall site density and/or intensity must comply with Table 3-1. All adaptive reuse projects are subject to the City of Covina design review process.

Prominent Architectural Design and Materials of the Food Arts Industrial Residential (F.A.I.R.) Zone

- 1 Varied building colors and materials
- 2 Multiple window/door openings
- 3 Defined entries
- 4 Pedestrian-oriented design at ground level
- 5 Adaptive reuse of industrial buildings
- 6 “Worn” appearance of streetscape condition preserved



Figure 3-8

Historic Core Zone

The **Historic Core Zone** is identifiable by its mix of retail stores, services, restaurants, cultural, entertainment venues, and residential uses within the Covina Town Center’s historic cultural and economic core. **New development capitalizes on the historical look and feel of the district** and attracts local and regional visitors to the area. **Protection of historic buildings is prioritized.** Compatible new development on vacant or underutilized properties that complements and enhances the historical look and feel of the district is essential. *Shared use of existing parking facilities* supports the rejuvenation of older structures. **Developments capitalize on alley connections** for improved accessibility and connectivity between the historic core and adjacent zones, while also encouraging public gathering opportunities.

The Historic Core Zone has a rich and eclectic mix of traditional buildings that embody Covina’s character. Numerous historic buildings and a comfortable pedestrian environment create a distinct identity. New development should respect the traditions by drawing on these qualities.

See Table 3-1 for permitted densities and intensities.

Prominent Architectural Design and Materials of the Historic Core Zone

- 1 Pedestrian-oriented design at ground level
- 2 Design statements and/or increased height at corners
- 3 Distinct entries
- 4 Abundant private space landscaping
- 5 Preservation of historical boulevard features
- 6 New construction compatible with existing historic building

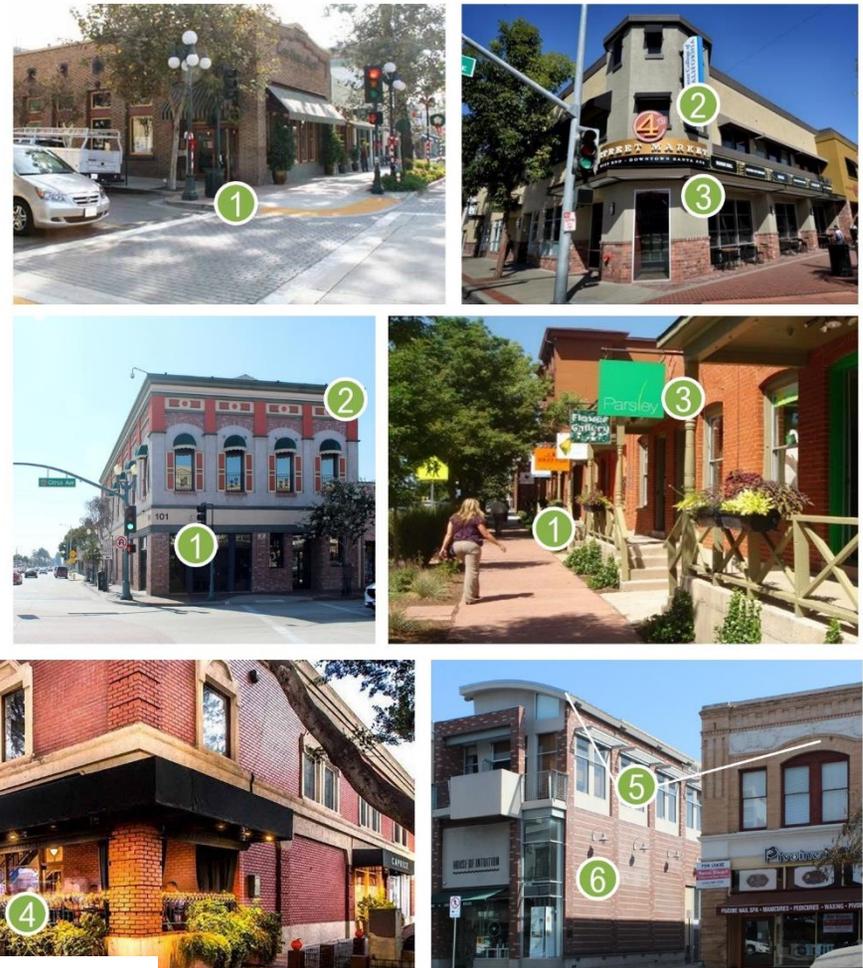


Figure 3-9

Civic Zone

The **Civic Zone** is for governmental and public land uses necessary to support community needs. Civic uses include public buildings, offices, or institutional uses owned and operated by governmental or other public agencies, as well as parks and open space. The concentration of government and institutional uses with regional clients attracts visitors and residents to the Covina Town Center. Civic/residential mixed use and civic/commercial mixed use are permitted. **Urban form with prominent public spaces** and pedestrian connections between adjacent land uses provides a safe and enjoyable experience for people. The creation of usable public spaces, **establishment of shared parking, and the use of alleys as placemaking opportunities** is encouraged. Urban design for new and redeveloped uses leverages and enhances the historical character of the Covina Town Center.

New civic use developments should be functional and durable and emphasize accessibility and high-quality construction and design that provide visual interest to the public realm with form and pedestrian connections between adjacent land uses.

Civic or governmental office uses are allowed to be located at ground level with residential above, thereby creating a vibrant day-night environment.

See Table 3-1 for permitted densities and intensities.



Figure 3-10

Prominent Architectural Design and Materials of the Civic Zone

- 1 Defined entry
- 2 Roof height variation
- 3 Modulated façade
- 4 Varied building colors and materials

Cultural Core Zone

The **Cultural Core Zone** is a vibrant area with a concentration of cultural and institutional land uses contributing to the vitality of the Covina Town Center through the attraction of local and regional visitors. The **establishment of new social/cultural uses** is prioritized while encouraging commercial uses and professional and administrative office uses that complement the character of the area and do not compete with uses in the Covina Town Center's Mixed-Use and Historic Core Zones. Design approaches emphasize a walkable urban form with **usable public spaces and pedestrian connections that invite people to walk** into the Covina Town Center's core. Alleys increase accessibility and walkability between the Covina Town Center and adjacent zones and can be repurposed into public gathering spaces.

Institutional uses with large parking lots are encouraged to enter into shared parking agreements with nearby commercial uses, thus reducing the need for surface parking lots in the Covina Town Center. Shared parking encourages the development of new uses and better accommodates visitors.

Similar to expectation for new Civic uses, durable and high-quality design in new cultural/institutional uses is important. Commercial uses and professional and administrative office uses in the Cultural Core designation should not compete with uses in the Covina Town Center's Mixed-Use and historic Downtown.

See Table 3-1 for permitted densities and intensities.

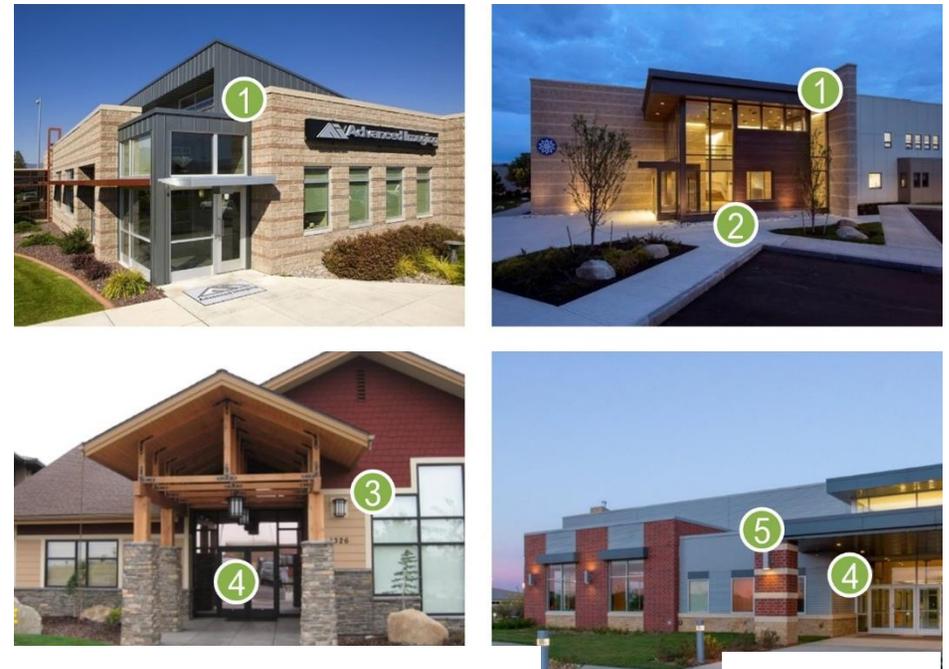


Figure 3-11

Prominent Architectural Design and Materials of the Cultural Core Zone

- 1 Roof height variation
- 2 Pedestrian-oriented design at ground level
- 3 Varied building colors and materials
- 4 Defined entry
- 5 Modulated façade

Medical Core Zone

The **Medical Core Zone** encourages the concentration of medical, health professional, and support-type uses, (including clinics, pharmaceutical, physician, and related business services and activities) that support the Emanate Health Inter-Community Hospital. New uses capitalize on the hospital’s investments and facilitate the growth of a medical-oriented area with uses that generate well-paying jobs for local residents. Development of vacant and underutilized sites is encouraged and supported. ***Efficient use and expansion of hospital uses is encouraged through replacement of surface parking areas with parking structures.*** Design features, gateway concepts, and wayfinding signage invite employees and visitors to walk into the Covina Town Center core. Alley connections and attractive pedestrian facilities such as “health corridors” increase accessibility and walkability between the Medical Core and adjacent districts. Residential uses are not allowed.

Medical uses should incorporate attractive and compatible design elements to blend in with the historical character of the Covina Town Center. Redevelopment of vacant and underutilized sites, such as surface parking lots, is encouraged.

See Table 3-1 for permitted densities and intensities.



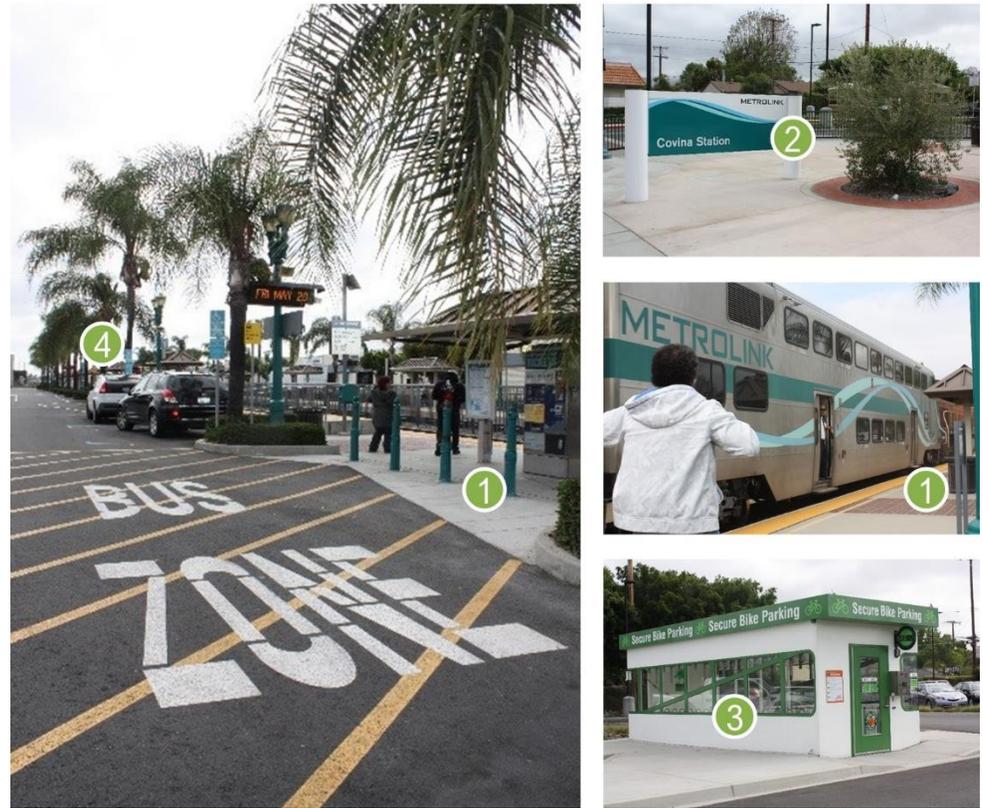
Prominent Architectural Design and Materials of the Medical Core Zone

Figure 3-12

- 1 Well-articulated façades
- 2 Varied colors and materials
- 3 Defined entry

Rail Zone

The **Rail Zone** is applied to the Covina Metrolink Station property and the rail rights-of-way. It facilitates the preservation and expansion of rail facilities including vehicle, bike, and other modes of transportation parking, commuter services, and transit center. See Table 3-1



Prominent Architectural Design and Materials of the Rail Zone

- 1 Pedestrian waiting area
- 2 Clear, easily-identifiable signage
- 3 Secure bicycle storage
- 4 Abundant landscaping

Figure 3-13

ALLOWABLE LAND USE BY ZONING DESIGNATION

Allowed land uses within the CTCSP are listed in **Table 3-2**. Certain uses may be subject to special conditions regarding the location, operation, design, or special permitting requirements of the use. Following an application submittal, the Community Development Director or his/her designee shall determine as to whether the proposed use is permitted, conditionally permitted, prohibited, or allowed as a temporary or accessory use to a permitted use.

- A Permitted Use (P) is allowed without discretionary approval and subject to all applicable provisions of this Specific Plan.
- A Conditionally Permitted Use (CUP) requires discretionary approval in the form of a Conditional Use Permit authorized by the Planning Commission and subject to the requirements outlined in CMC Chapter 17.62.
- An accessory use (A) refers to a use that is customarily incidental and subordinate to a primary use of the land or building and located on the same lot with the primary use or building.
- Any use not specifically listed in Table 3-2 shall be interpreted as not allowed in the Covina Town Center.
- Uses specifically not allowed in this Specific Plan are indicated by (—).

It is not possible to anticipate every land use that might be proposed and suitable in the future. Uses not listed in the table, unless determined to be substantially similar¹ to a listed use, following an application for use determination, the Director of Community Development or his/her designee shall decide whether the proposed use is permitted, conditionally permitted, prohibited, or allowed as a temporary or accessory use to a permitted use. The use determination by the Director is final unless an appeal is received within 10 calendar days from the decision. The Director may, at his/her discretion, refer the use determination request to the Planning Commission or the City Council as set forth in the CMC Chapter 17. Prohibited uses are listed at the end of this chapter. All existing nonconforming uses that are listed as prohibited shall be subject to the procedures outlined in Chapter 6. Detailed definitions for uses are listed in Chapter 7.

¹ Substantially similar is defined as a use producing a similar product, having similar hours of operation, or having similar operation equipment or procedures as an use defined in the CTCSP.

Table 3-2 Allowable Land Use (Revised: 9-1-2020; 9-20-2022; 4-18-23)

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A = Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail
	ZONES											ADDITIONAL REGULATIONS/NOTES		
	RTC-W	RTC-E	N	T/H	MU	F ²	HC	C	CC	MC	R			
Accessory Uses														
Private outdoor patios, decks, rooftop decks	P	P	P	P	P	P	P	P	P	P	P	-	Subject to CMC Chapter 17.64 Site Plan Review – Director’s Approval.	
Public or Private EV Charging Stations or Centers	P	P	P	P	P	P	P	P	P	P	P	P	Subject to CMC Chapter 17.64 Site Plan Review – Director’s Approval.	
ATM facilities	-	-	-	A	A	A	A	A	A	A	A	-	Subject to CMC Chapter 17.64 Site Plan Review - Director’s Approval.	
Smart Parcel Lockers	-	-	-	A	A	A	A	A	A	A	A	A	Subject to CMC Chapter 17.64 Site Plan Review – Director’s Approval.	
Eating, Entertainment, and Drinking Establishments														
Alcohol Sales, On-Site with or without Food Service.	-	-	-	-	CUP	CUP	CUP	CUP	CUP	-	-	-	Includes full service restaurants (see CMC Chapter 17.04.100), bars, lounges, taverns, brew-pubs, food to go/take out establishments and other uses similar in nature and operation. Excludes nightclubs and establishments with dancing. Alcohol sales area exceeding 2,000 square feet of gross floor area shall be considered as bar or lounges and not as bona fide/full service restaurants. Subject to provisions of CMC Chapter 17.62.026.	
Commercial Kitchen, Culinary Education and Retail Kitchenware store	-	-	-	-	P	P	P	-	-	-	-	-	Comply with Chapter 7 of CTCSP.	
Hospital Cafeteria (without alcohol)	-	-	-	-	-	-	-	-	-	P	-	-		
Establishments with Live Entertainment, including Dancing (with or without alcohol)	-	-	-	-	CUP	CUP	CUP	-	-	-	-	-	Comply with CMC Chapter 5.28: Entertainment.	

² Require a CUP for applying the TOD/HDR density, design and development standards to residential project.

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail	
	ZONES											ADDITIONAL REGULATIONS/NOTES			
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R				
Eating, Entertainment, and Drinking Establishments															
Entertainment and Performance Establishments (without alcohol)	-	-	-	-	P	P	P	P	P	-	-	Subject to the provisions of CMC Chapter 5.28: Entertainment. Excludes establishment in which alcoholic beverages are sold, served, or given away for consumption on-site. See Alcohol Sales, On-Site with or without Food Service.			
Entertainment and Performance Arts Theatre with ancillary uses such as but not limited to banquets, rental of theatre, special events, and on-site alcohol							CUP					Subject to the provisions of CMC Chapter 5.28: Entertainment. Comply with CMC Chapter 17.62. See Chapter 7 of CTCSP - Alcohol Sales, On-Site with or without Food Service.			
Indoor Recreation Facilities						CUP	CUP	CUP	CUP			Comply with CTCSP Chapter 7 Glossary. Comply with CMC Chapter 17.62 for On-site Alcohol Sales			
Outdoor Recreation Facilities						CUP	CUP	CUP	CUP			Comply with CTCSP Chapter 7 Glossary. Comply with CMC Chapter 17.62 for On-site Alcohol Sales			
Retail Establishments with ancillary food or beverage service (with or without alcohol)	-	-	-	P	P	P	P	P	P	-	-	Comply with CMC Chapter 17.62. See Chapter 7 of CTCSP - Alcohol Sales, On-Site with or without Food Service.			
Restaurant, Full-Service, Food To Go/Take-out, or Fast Food (without alcohol)	-	-	-	P	P	P	P	P	P	-	-				
Restaurant, Full-Service, Food To Go/Take-out, or Fast Food (with alcohol)	-	-	-	CUP	CUP	CUP	CUP	CUP	CUP	-	-	Comply with CMC Chapter 17.62 . See Chapter 7 of CTCSP - Alcohol Sales, On-Site with or without food Service.			
Drive Through or Drive Up Facilities	-	-	-	-	CUP	-	-	CUP	CUP	-	-				

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail	
	ZONES											ADDITIONAL REGULATIONS/NOTES			
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R				
Eating, Entertainment, and Drinking Establishments															
Outdoor Dining (with alcohol)	-	-	-	CUP	-	-	Comply with Chapter 7 of CTCSP - Alcohol Sales, On-Site with or without Food Service. Subject to provisions of CMC Chapter 11.26: Outdoor Sidewalk Dining and CMC Chapter 17.57: Outdoor Dining and Display. Parking for outdoor dining areas is required only for outdoor seats in excess of 25 or a number of outdoor seats equivalent to 50% of the number of indoor seats, whichever is less.								
Outdoor Dining (without alcohol) within Private Property	-	-	-	P	P	P	P	P	P	P	-	-	Subject to provisions of CMC Chapter 11.26: Outdoor Sidewalk Dining and CMC Chapter 17.57: Outdoor Dining and Display. Parking for outdoor dining areas is required only for outdoor seats in excess of 25 or a number of outdoor seats equivalent to 50% of the number of indoor seats, whichever is less.		
Outdoor Dining (without alcohol) within Public Right-of-Way	-	-	-	ACUP	-	-	Subject to provisions of CMC Chapter 11.26: Outdoor Sidewalk Dining and CMC Chapter 17.57: Outdoor Dining and Display. Parking for outdoor dining areas is required only for outdoor seats in excess of 25 or a number of outdoor seats equivalent to 50% of the number of indoor seats, whichever is less.								
Pop up Food Trucks or Pods (with alcohol) stationed on private property for more than 180 days						CUP	CUP	CUP	CUP						
Pop up Food Trucks or Pods	-	-	-	-	ACUP	P	ACUP	ACUP	ACUP	ACUP	-	ACUP			

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail	
	ZONING DESIGNATIONS											ADDITIONAL REGULATIONS/NOTES			
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R				
Eating, Entertainment, and Drinking Establishments															
Shared Space Retail/Service (Food Halls)	-	-	-	p	P	P	P	P	P	-	-	Prohibit swap meets, flea markets, open-air markets, or other similar terms. The primary characteristic shared by these events involves used personal property being offered or displayed by temporary vendors within a shared space. The vendors engage in a series of sales sufficient in number, scope, and character constituting a regular course of business (based on California’s Business and Professions Code, Division 8, Chapter 9, Article 6, 21661). Comply with CMC Chapter 17.62.026 for regulations and standards for alcohol sales. A CUP for on-site consumption of alcohol is required either in the form of a master CUP obtained by the owner/operator of the Shared Space Retail/Service establishment and covering all the lessees in the establishment or a CUP for an individual business leasing a space.			

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use	F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail
	ZONES											ADDITIONAL REGULATIONS/NOTES	
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R		
INDUSTRIAL													
Boutique Winery, Brewery and Distill Alcohol Production and On-Site Tasting. May have ancillary uses such as retail sales, food trucks, food services, public assembly, and special event.	-	-	-	-	CUP	CUP	CUP	CUP	CUP	-	-	Comply with CMC Chapter 17.62.026 for regulations and standards for alcohol sales. Calculations of gross floor area for alcohol sales and entertainment establishment regulations exclude areas used for alcohol production that are not accessible to the public.	
Industrial, Light	-	-	-	-	-	P	-	-	-	-	-	Automobile Sales and Services are not allowed. Automobile Sales and Service uses that were lawfully established and approved before the effective date of adoption or amendment of this Specific Plan, but which no longer comply, may be continued so long as such use remains otherwise lawful. Any update or upgrade of the use is subject to a CUP and will require full compliance with this Specific Plan.	
Outdoor Storage	-	-	-	-	-	A	-	-	-	A	-	Comply with CMC Chapter 17.54.170 for additional screening regulations. Must be screened from public right-of-way view.	
Research and Development	-	-	-	-	-	P	-	-	-	-	-		
Indoor Food Production (Urban Agriculture, Vertical Farms)						CUP						Comply with Chapter 7 Glossary	
INSTITUTIONAL AND PUBLIC FACILITIES													
Cultural/Civic Facilities (Non-Governmental)	-	-	CUP	-	P	P	P	P	P	P	-		
Government Facilities	-	-	CUP	-	P	P	P	P	P	P	-		

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail	
	ZONES											ADDITIONAL REGULATIONS/NOTES			
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R				
INSTITUTIONAL AND PUBLIC FACILITIES															
Open/Public Space, Usable or Improved including public or private plazas, greenways, and paseos.	-	-	-	P	P	P	P	P	P	P	P	P			
Parking Facilities	-	-	-	P	P	P	P	P	P	P	P	P			
Parks and Recreation Facilities	-	-	-	P	P	P	P	P	P	P	P	-			
Parklets	-	-	-	ACUP	-										
Schools (Public and Private)	-	-	-	-	-	P	-	P	P	P	P	-			
Trade and Vocational Schools	-	-	-	-	-	P	-	P	P	P	P	-			
Transit Station	-	-	-	-	-	-	-	-	-	-	-	P			
Utilities	-	-	-	-	P	P	P	P	P	P	-	-	Comply with Chapter 7 Implementation and CMC Chapter 17.65 Wireless Communications Facilities.		

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use			F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail		
	ZONES											ADDITIONAL REGULATIONS/NOTES					
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R						
MEDICAL RELATED USES																	
Medical Clinics	-	-	-	-	CUP	-	-	-	-	P	-						
Medical and Dental Offices	-	-	CUP	-	-	-	-	-	-	P	-	Ground floor office uses are only allowed in the Medical Core designation.					
Hospitals	-	-	-	-	-	-	-	-	-	P	-						
Laboratories, Medical or Dental	-	-	-	-	CUP	CUP	-	-	-	P	-						
Pharmacies	-	-	CUP	-	P	CUP	-	-	-	P	-						
Veterinary Services (Animal Hospital/Clinic) with boarding and/or daycare	-	-	-	-	P	P	-	-	-	P	-	Temporary boarding allowed only in association with medical or surgical treatment.					
MIXED USES																	
Civic/Residential Mixed Use	-	-	-	-	-	-	-	P	-	-	-						
Civic/Commercial Mixed Use	-	-	-	-	-	-	-	P	-	-	-						
Mixed Use	-	-	-	P	P	P	P	P	-	-	-	Comply with CMC Chapter 17.64.250 B. Exemption from CMC Chapter 17.64.190, Traffic Impact Analyses. As specified in the Los Angeles County congestion management program manual, certain development projects are exempted from conducting a congestion management program traffic impact analysis, as follows: Mixed-use development located within one-half mile of the Metrolink, if more than half of the land area, or floor area, of the mixed-use development is used for high density residential housing, as determined by Director of Community Development.					

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use	F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail
	ZONES											ADDITIONAL REGULATIONS/NOTES	
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R		
RECREATION AND PUBLIC ASSEMBLY USES													
Assembly/Meeting Facilities – Private	-	-	-	-	CUP	CUP	CUP	P	CUP	-	-		
Commercial Recreation Facility – Indoor	-	-	-	-	P	P	P	-	P	-	-	Includes gyms, “escape rooms”, video arcade, ice- or roller-skating rinks, trampoline, bounce house establishments, and rock climbing. Require CUP for on-site consumption of alcohol.	
Special/Temporary Events	-	-	ACUP	-	Consistent with business license/permit requirements in CMC Chapters 5.08 and 11.48.								
RESIDENTIAL													
Co-Living	-	-	-	-	-	P	-	-	-	-	-		
Live/Work Unit	-	-	P	P	P	P	P	P	P	-	-		
Multiple-Family Dwelling	P	P	-	P	P	P	P	P	P	-	-	Comply with CMC Chapter 17.64.250. Exemption from CMC Chapter 17.64.190, Traffic Impact Analyses. As specified in the Los Angeles County congestion management program manual, certain development projects are exempted from conducting a congestion management program traffic impact analysis, as follows: High density residential development located within one-fourth mile the Metrolink station; Ground floor residential is not allowed in the Historic Core.	

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail	
	ZONES											ADDITIONAL REGULATIONS/NOTES			
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R				
RESIDENTIAL															
Single-Family Dwelling	P	P	P	-	-	-	-	-	-	-	-	-	-		
Two-Family Dwelling (Duplex)	P	P	P	-	-	-	-	-	-	-	-	-	-		
Single Room Occupancy Residential (SRO)	-	-	-	-	CUP	P	CUP	-	-	-	-	-	-		
Small and Large Family Day Care/Licensed facilities	P	P	P	P	P	P	P	-	P	-	-	-	-	Small family day cares is up to 6 children. Large family day care is 7 to 14 children.	
Small Group Home/Licensed Facilities	P	P	P	P	P	P	P	-	P	-	-	-	-	Small group home is 6 or fewer persons	
RETAIL USES															
Antique Store/Shop	-	-	-	-	P	P	P	-	-	-	-	-	-	Comply with CMC Chapter 17.04.051 and Chapter 7 of CTCSP.	
Arts and Crafts Sales and Galleries	-	-	CUP	P	P	P	P	P	P	-	-	-	-		
Grocery Store, Small (less than 10,000 Sq. Ft.)	-	-	-	CUP	P	CUP	P	-	CUP	-	-	-	-	Require CUP for off-site consumption of alcohol.	
Grocery Store, Large (Supermarket) (10,000 Sq. Ft. +)	-	-	-	CUP	CUP	CUP	-	-	-	-	-	-	-	Require CUP for off-site consumption of alcohol.	
Cannabis Businesses	-	-	-	-	CUP	CUP	-	-	-	-	-	-	-	Comply with CMC Chapter 17.62 CUP, CMC Chapter 5.80 Cannabis Business Permit, and CMC Chapter 17.84 Cannabis Prohibitions and Regulations. Comply with CTCSP Chapter 3, Figure 3.2 for locational boundary in downtown area prohibiting cannabis retailers and Chapter 7 Glossary	
Retail Store (less than 10,000 Sq. Ft.)	-	-	-	P	P	P	P	-	-	CUP	-	-	-		
Retail Store (10,000 Sq. Ft.+)	-	-	-	CUP	P	CUP	-	-	-	-	-	-	-		
Specialty Food Store, at least 1,000 Square feet in gross floor area.	-	-	-	-	P	P	P	P	P	P	P	-	-	Comply with Chapter 7 of CTCSP.	
Specialty Store	-	-	-	P	P	P	P	-	-	P	-	-	-	Comply with Chapter 7 of CTCSP.	

USES	P = Permitted by Right CUP = Conditional Use Permit ACUP = Administrative Conditional Use Permit A= Permitted as an Accessory Use — = Not Allowed											RTC-W = Residential Town Center West RTC-E = Residential Town Center East N = Neighborhood T/H = TOD/HDR MU = Mixed Use		F = F.A.I.R HC = Historic Core C = Civic CC = Cultural Core MC = Medical Core R = Rail	
	ZONES											ADDITIONAL REGULATIONS/NOTES			
	RTC-W	RTC-E	N	T/H	MU	F	HC	C	CC	MC	R				
SERVICE USES															
Animal Boarding/Kennels/Grooming	-	-	-	-	CUP	CUP	-	-	-	-	-	-	-		
Banks/Financial Institutions	-	-	-	P	P	P	P	P	P	P	-	-	Drive-through facility requires a CUP.		
Barber shops, Beauty Salons including Hair, Nails and Facial (neck up only)	-	-	-	P	P	P	P	P	P	P	-	-	Massage establishment prohibited		
Body Art	-	-	-	-	-	CUP	-	-	-	-	-	-	Comply with CMC Chapter 17.67.		
Business and Consumer Support Services	-	-	-	-	P	P	P	P	P	P	-	-	Comply with Chapter 7 Glossary of CTCSP.		
Health/Fitness Facility	-	-	-	CUP	P	P	P	-	-	P	-	-			
Office (General) including maker spaces, co-working spaces, conference spaces, and network mixer spaces	-	-	CUP	CUP	P	P	P	-	P	-	-	-			
Personal Services (General)	-	-	-	CUP	CUP	CUP	CUP	-	-	-	-	-	Comply with Chapter 7 Glossary of CTCSP.		
Studio – Art, Dance, Music, Sound, Video	-	-	CUP	CUP	P	P	P	-	-	-	-	-			
Specialty Bar and Cigar Lounge, at least 1,500 square feet in floor area	-	-	-	-	-	CUP	CUP	-	-	-	-	-	Comply with CMC Chapter 17.61. On-site consumption of alcohol subject to a CUP. Comply with CMC Chapter 5.28 for entertainment. Comply with Chapter 7 of CTCSP.		
VISITOR ACCOMMODATIONS															
Bed & Breakfast Inn or Lodge	-	-	-	-	-	CUP	CUP	-	-	-	-	-	Comply with CMC Chapter 17.62.027 for additional standards.		
Hotel	-	-	-	-	CUP	-	CUP	-	-	-	-	-	Comply with CMC Chapter 17.62.027 for additional standards.		
Motel	-	-	-	-	CUP	-	-	-	-	-	-	-	Comply with CMC Chapter 17.62.027 for additional standards.		

PROHIBITED USES (Revised: 9-1-2020; 9-20-2022; 4-18-23)

The following uses, as defined in **Chapter 7: Glossary**, are **prohibited within the CTCSP area**:

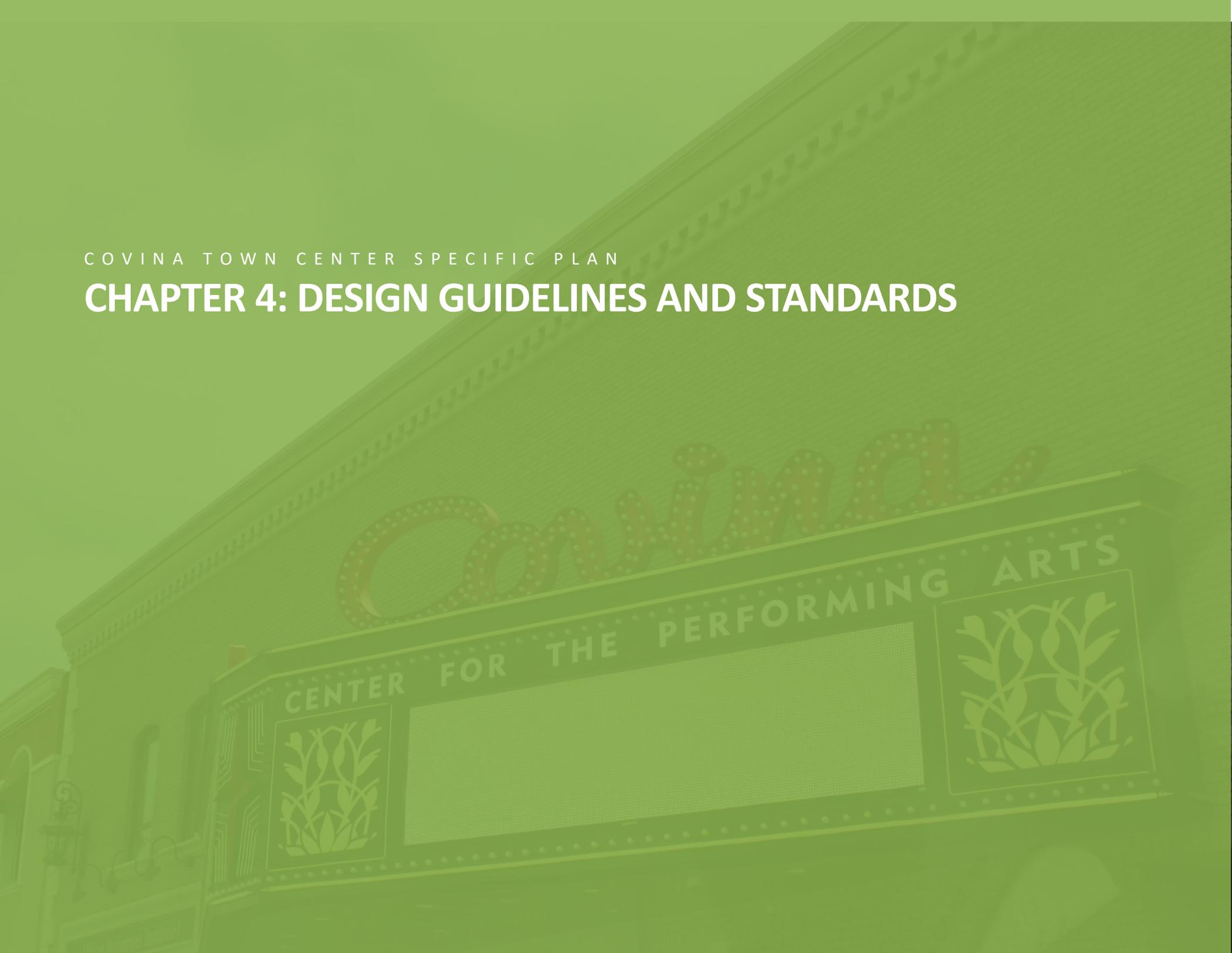
- Adult-Oriented Businesses
- Auto Sales and Services
- Boarding/Rooming Houses
- Cannabis Cultivation (indoor/outdoor) per CMC Chapter 17.84
- Cannabis Distribution and Delivery per CMC Chapter 17.84
- Cannabis Manufacturing per CMC Chapter 17.84
- Check Cashing/ Payday Loan Use
- Commercial Cannabis Activity (medical/nonmedical) per CTCSP Chapter 3, Figure 3-2, unless otherwise specifically listed on page 3-31 and subject to compliance with CMC Chapter 5.80, Chapter 17.62, and Chapter 17.84
- Contractor Yards
- General Personal/Mini Storage
- Indoor Swap Meets
- Industrial, Heavy
- Massage Parlor
- Outdoor Storage, unless otherwise specifically listed on page 3-27
- Pawn Shops
- Service/Gas Stations
- Smoke/Tobacco Shops, unless otherwise specifically listed on page 3-32
- Stand Alone Recycling Centers
- Stand Alone Wireless Communication Facilities
- Thrift shops, secondhand, and used merchandise stores (specialty stores that sell one type of used item such as books or records is allowed under Specialty Store)
- Vehicle Storage (Primary)
- Warehouse/Storage

All existing nonconforming uses that are listed as prohibited shall be subject to the procedures outlined in **Chapter 6 (Expiration of Prohibited Nonconforming Uses)** of this Specific Plan.



COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 4: DESIGN GUIDELINES AND STANDARDS





Chapter 4 Contents

- Introduction
- Purpose
- Public Realm Design Guidelines and Standards
- Private Realm Design Guidelines and Standards

DESIGN GUIDELINES AND STANDARDS

INTRODUCTION

An integrated and well-designed public realm is critical to creating a memorable, accessible, and economically vibrant Town Center with a distinct sense of place. The character of landscape plantings, lighting, street furniture, and other streetscape components shape human perceptions and can serve to draw people into Covina's city core. Exterior architectural features and building placement on a site are critical in defining the character of a place. This chapter's design standards and guidelines provide a framework for the Town Center's public improvements, site design, and building architecture. Stewardship of these critical design components will help leverage the area's existing strengths and pedestrian scale to create a more unified and welcoming Town Center. The improved streetscape will invite more activity and complement adjacent land uses. Streetscape improvements will be implemented over time and may be carried out by various developers as well as the City of Covina. Ultimately, these improvements will help **enhance local business vitality, promote economic development, foster entrepreneurship and creativity, and bolster sense of character and place for all of Covina.**

PURPOSE

The purpose of the Design Guidelines and Standards is to provide greater clarity and certainty about project design standards, site planning and architectural design expectations for the project applicants and the community. The goal is to expedite the review process by clearly stating the City's desires for high quality projects that contribute to creating a more memorable, accessible, economically vibrant, and regionally attractive Downtown.

The Design Guidelines and Standards are based on the following guiding principles:

- A development project is to comply with the standards and intent of the guidelines. In the event of a conflict between the standards in the Zoning Code and the Covina TCSP standards and guidelines, these standards and guidelines shall apply.
- A development project is not entitled to the maximum density allowed under the zoning designation if the project cannot comply with the design guidelines and standards. On many small and/or narrow lots, the potential density may be lower than the maximum allowed under the zoning designation. A narrow lot is not a basis for approving exceptions to standards and guidelines. It is a guide for City staff and/or City's decision makers to determine and make the findings that the proposed development project has complied with the design guidelines and standards, prior to approval.

Diagrams and Photos

Diagrams and photographs shown in this document are intended to illustrate certain standards or present particular aspects of design.

Applicants

Applicants should meet with City staff early in the design process for assistance with the interpretation of the Design Guidelines and Standards and their application to a specific site or project.

GUIDELINES

Guidelines express objectives with respect to specific development features or conditions and explain why a particular guideline or criterion is an appropriate way to achieve the objective. Guidelines provide graphic examples and written description showing approaches that conform to the guidelines. These guidelines augment the standards in this document and provide qualitative direction on how to meet the Town Center's goal of high-quality design.

STANDARDS

Standards are qualitative or quantifiable rules or measures that must be satisfied.

PUBLIC REALM DESIGN STANDARDS AND GUIDELINES

Sidewalk, Street Trees, and Crosswalk/Treatments

Safe, accessible, and well-designed sidewalks and crosswalks are essential for an activated Town Center setting, allowing people to “park once” and easily visit local businesses and community facilities on foot. Paving should be simple and consistent throughout the Town Center to allow for seamless connectivity between destinations, the Covina Metrolink Station, and nearby parking.

Sidewalk Paving

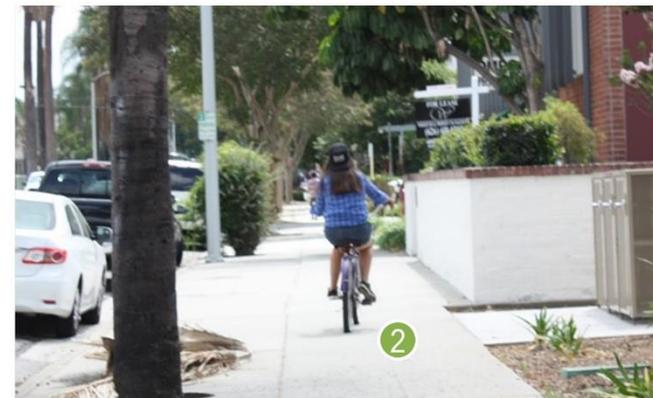
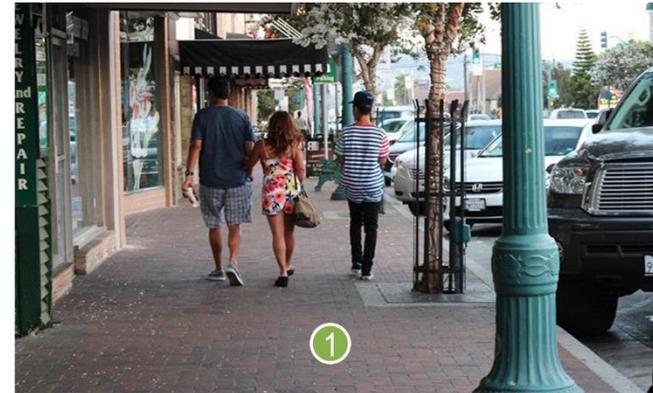
Standards

- Citrus Avenue: Install interlocking brick pavers (terra cotta/brown mixed). Sidewalks shall meet City of Covina standards for public works, which include ADA-compliant accessibility.
- Health Corridor: Install sidewalks and/or paving materials that accommodate all users including wheel chairs and scooters, paying special attention to ADA-compliant accessibility. Consider installing decorative paving to break up the sidewalks into different speeds of travel, for example, pedestrians, wheelchairs/scooters, and other mobility aids.
- All Other Locations: Install standard gray Portland cement concrete sidewalks (including curb ramps at all intersections) to meet City of Covina standards including ADA-compliant accessibility.

Guidelines

- Sidewalk paving should incorporate recycled content to enhance sustainability.
- Sidewalk paving adjacent to bus stops should be a minimum of 15’ wide (in accordance with Metro best practices) or meet City of Covina standards, whichever is greater.

Figure 4-1



1 Interlocking brick pavers (terra cotta brown/brown mixed), per City of Covina Standards

2 Grey Portland cement concrete per City of Covina Standards

Street Trees Standard

All trees planted in any public street shall conform as to species, spacing, and location to the City of Covina Master Street Tree Plan and Reports, which by reference is made a part of this chapter and is on file with the City.

Crosswalks and Crosswalk Treatment Standard

- Citrus Avenue - At the key intersections listed below, install crosswalks with stamped or scored concrete, designed to form a 24" square grid (terra cotta and/or brown color mix), lined by a 12" gray concrete band running the length of the crosswalk.
 - North Citrus Avenue and East Front Street
 - North Citrus Avenue and San Bernardino Road
 - North Citrus Avenue and West Cottage Drive/East Italia Street
 - North Citrus Avenue and College Street
 - Citrus Avenue and Badillo Street
- Civic and Cultural Core – At key intersections, listed below, install creatively painted crosswalks.
 - North 2nd Avenue and East College Street
 - North 2nd Avenue and East Italia Street
 - North 3rd Avenue and West College Street
 - North 3rd Avenue and West Cottage Drive
- Health Corridor – Install enhanced crossing along West Cottage Drive and East College Street to increase user safety and account for the needs of all user groups.
- 3rd Street between College and Orange- create a “Health Plaza” using decorative colored stamped or scored concrete bordered by a 12" grey concrete band surrounding the plaza.
- All Other Locations - Continental style crosswalks

Figure 4-2



- 1 Stamped or scored concrete
- 2 Creatively painted crosswalks, continental style

Street Furniture and Lighting Standards

Street furniture invites people to linger outside and encourages social activity by making the public realm more comfortable and convenient. Pedestrian-scaled street lighting enhances safety, encourages evening use of outdoor spaces, and contributes to aesthetics. Street furniture and lighting standards will **improve the appeal and consistency of the Town Center streetscape**. These standards do not preclude placement of street furniture elsewhere on the sidewalk, if desired by the City.

- Provide consistent street furniture per City standard in the public right-of-way throughout the Covina Town Center Specific Plan (CTCSP) area. Approved street furniture elements include benches, trash receptacles, water fountains, bike racks, bollards, and lighting:
 - Citrus Avenue District- Continue to use current Citrus Avenue street furniture design.
 - Front Street/TOD/F.A.I.R District - Street furniture should be industrial in nature to reflect the existing character. Explore opportunities for artistic design of street furniture, bike racks, and bus stops.
 - Civic and Cultural Core District – Street furniture should reflect culture and art to reflect the unique uses of the area. Explore opportunities for artistic design of street furniture, bike racks, and bus stops.
 - All Other Districts – Continue to use current City street furniture design.
- Place benches, trash/recycling receptacles, and bicycle racks on the sidewalk where they will not interfere with pedestrian movement. Place benches in naturally shaded areas, whenever possible.
- Space pedestrian lighting approximately 40' apart.
- Install pedestrian lighting in conjunction with new sidewalks as part of future streetscape improvement projects. Identify pedestrian lighting that is high efficiency and low-glare. The use of LED lighting is highly encouraged.
- Use planters, plant pots, and other landscaping elements to provide greenery along sidewalks where street trees cannot be accommodated.

Street Furniture and Lighting Standards | Citrus Ave District

Figure 4-3



Pedestrian Bench
Rough&Ready All Black Benches |
StreetLife



Trash Receptacles
The Central Park Conservancy
Recycling System | Landscape Forms



Bicycle Rack
Key Bike Rack | Landscape Forms



Bollard
Annapolis Bollard | Landscape Forms



Pedestrian Light
Standard
Concord -Alcott |
Landscape Forms

Street Furniture and Lighting Standards | Cultural Core

Figure 4-4



Pedestrian Bench
Plainwell (bright color) | *Landscape Forms*



Trash Receptacles
The Central Park Conservancy
Recycling System | *Landscape Forms*



Bicycle Rack
Key Bike Rack (bright colors) |
Landscape Forms



Bollard
Annapolis Bollard | *Landscape Forms*



Pedestrian Light Standard
Concord – Alcott | *Landscape Forms*



Street Furniture and Lighting Standards | Front Street/ TOD/ F.A.I.R. District

Figure 4-5



Pedestrian Bench
Rough&Ready All Black Benches |
StreetLife



Trash Receptacles
Poe Litter Receptacle (black) |
StreetLife



Bicycle Rack
Key Bike Rack (black and red) |
StreetLife



Bollard
Rough&Ready All Black Bollards |
StreetLife



Pedestrian Light Standard
Rama | Landscape Forms

Gateways

Specialty gateway elements demarcate the Town Center area to visitors and contribute to the Town Center’s image and identity.

Primary gateways are located at:

- North Citrus Avenue/East Front Street (Front Street/TOD/F.A.I.R. District)
- Citrus Avenue/Badillo Street

Secondary gateways are located at:

- North 4th Avenue/West San Bernardino Road
- 4th Avenue/West Badillo Street
- Barranca Avenue/East Front Street (Front Street/TOD/F.A.I.R. District)
- Barranca Avenue/East San Bernardino Road
- 1st Street/East Badillo Street

Figure 4-6



Standard

- Gateway lighting shall have internal illumination for nighttime visibility.
- All primary and secondary Gateway structures and elements (except for the Front Street/TOD/F.A.I.R. Districts gateway) shall have the same aesthetic character and quality as the other Town Center gateway. The gateway structures and elements may be identical, if desired.
- Front Street/TOD/F.A.I.R. District gateways
 - The two gateways within the Front Street/TOD/F.A.I.R. District shall reference the Town Center's gateways, yet be visually distinctive in terms of design, color, and character. The Front Street/TOD/F.A.I.R. District's gateway structures and elements should emphasize the unique industrial, "maker's district" nature of the District.

Guidelines

- **Primary gateways**
 - Form a partial or full span arch-type structure crossing the street, framing the views into the Town Center. The gateways' character should be a contemporary design that evokes a civic, town center feel, and be relatively airy (with openings) rather than appear massive
 - Paint steel arch with lettering ("Town Center").
 - Construct supporting poles with either painted steel (to match arch) or dark gray concrete.
- **Secondary gateways**
 - Form either a partial span arch-type structure or vertical pylon (approximately 8'-10' in height) flanking the predominant street.
 - Design should be consistent with the primary gateways in terms of character, materials, and lettering.
 - Construct supporting poles with either painted steel (to match arch) or dark gray concrete.

Gateways

- 1 Incorporate full-span arches and/or vertical pylons.
- 2 Use contemporary "town center" design
- 3 Select distinct, identifying text.
- 4 Design "airy" gateways with openings.



Public Art Guidelines (revised 9-20-2022)

Public art provides elements and spaces for the community to engage, create dialogue, educate, foster enjoyment, celebrate culture and history, or establish identity and values. Public art provides the opportunity to **create a unique place within the Town Center and attract tourism and economic investment.**

Public art should:

- Maintain the primary purpose of the display of visual art, including but not limited to paintings, murals, prints, photographs, fountains, and sculptures. Public art shall not be commercial in nature.
- Be an original, creative work by an individual or group.
- Be placed in a highly visible and accessible location either on public or private property.

Figure 4-8



Examples of highly visible, publicly accessible, creative, original, public art.



Curb Extensions (Bulb-outs)

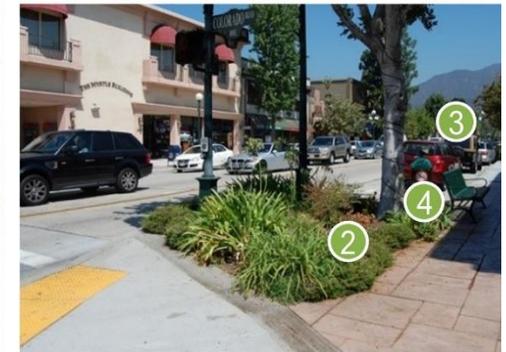
Curb extensions **enhance pedestrian safety and reduce vehicle speeds** by narrowing the roadway and shortening the crossing distances. Curb extensions also provide opportunities for additional streetscape elements such as stormwater planters, additional seating, bike parking, or public art.

Standards

- Provide additional pedestrian amenities such as landscaping, seating, trash receptacles and bicycle racks in areas with heavy pedestrian traffic.
- Use stormwater planters to collect and treat storm water runoff in areas with low pedestrian traffic.
- Design curb cuts to have a flush ramp condition for pedestrians to transition from the sidewalk to the street.

Guideline

- Encourage public art or interpretive elements to be included within the curb extensions, where possible.



- 1 Flush ramp that's ADA compliant
- 2 Bulb-out landscape and/or stormwater planters
- 3 Protects parking lane
- 4 Seating

Parklets

Figure 4-9

Parklets repurpose small street segments into public spaces. These small parks, generally located in on-street parking lanes, provide seating, planting, bicycle parking, and art amenities. Parklets encourage social activity by creating community spaces where people can **move out of pedestrian traffic, dine, or simply people watch, creating a more exciting pedestrian realm.** Parklet guidelines aim to create a series of small intimate spaces along the street. The design of any individual parklet may vary according to the wishes of the primary partner or applicant. Designs may include seating, greenery, bicycle racks, or other features, but should strive to become a focal point for the community and a welcoming public gathering place.

Standards

- Design decking to be flush with the curb (not more than a ½" gap from the curb). If this is not possible, the parklet must be ADA accessible.
- Maintain a 36"- wide, minimum, ADA accessible entryway to the parklet.
- Design the sidewalk-facing side to open to pedestrians, while the street-facing side should provide a barrier not less than 3' in height to create a boundary for the space.
- Do not exceed the width of the parking lane in which the parklet is placed. If parking lane is not striped, consult with the Public Works Department to obtain official parking lane width.
- Install outer-edge railing a minimum of 18" from the travel lane, creating, at minimum, an 18" clear.
- Avoid tree wells when designing/installing the sidewalk-facing entrance.

- Incorporate vertical elements (e.g., planters, umbrellas) to be visible to passing vehicles. However, these must not obstruct driver views.

Guidelines

- Encourage multiple points of entry along the curbside
- Finish parklets with high-quality materials and include amenities, especially permanent seating, integrated into the parklet structure. This ensures the parklet feels welcoming after moveable furniture (tables and seating) are taken inside at night.

Parklets

- 1 Sidewalk facing side open to pedestrians
- 2 Shade structure
- 3 Vertical elements
- 4 Street-facing side includes 3' minimum barrier to vehicles
- 5 18" minimum distance to travel lane
- 6 Multiple points of entry
- 7 Decking flush with curb
- 8 Landscaping
- 9 Interesting design shape and color



Figure 4-10



Plaza Design/Community Activity Area

Plazas help to **enliven commercial centers and add visual interest**. Intimately scaled outdoor areas or plazas should accommodate daily activities (e.g., eating and relaxing) and create space for public events (e.g., civic ceremonies, festivals, and live performances). Plazas should incorporate shade, rest areas, and other amenities such as drinking fountains, water features, trashcans, accent lighting, public art, or other similar enhancements that encourage use and social gathering.

Standards

- Use fountains, water features, public art, or wind chimes or other auditory elements to add interest to plaza areas.
- Provide seating in proximity to deciduous trees that offer shade from summer sun and access to winter sunlight.
- Provide vertical plantings along walls abutting the public spaces.
- Integrate pedestrian amenities (i.e., site furniture, shade structures, seating, etc.) into the overall unified project site design.
- Incorporate enhanced paving within plazas and outdoor spaces consistent with the Town Center design theme and architectural style.
- Provide clearly visible pedestrian connections.
- **Note:** Plaza Designs and Community Activity Areas are required to incorporate a minimum of four of the five above design standards or guidelines in their proposed developments. Applicability of incorporated design elements is subject to City Staff review and determination.

Guidelines

- Arrange and orient structures on the sites to create well defined, intimate, and pedestrian-friendly common plaza spaces.
- Design raised landscape planters to allow for seating but discourage undesirable activities, (e.g., skateboarding, other stunts).
- Enhance pedestrian connections with decorative paving, landscaping, decorative trellises, and/or arbor features.
- Use landscaping to enhance and define the various plaza uses.

Plaza Design/Community Activity Area

- 1 Well-defined common plaza space
- 2 Seating near trees
- 3 Decorative trellis
- 4 Raised structures providing seating
- 5 Pedestrian amenities (tables, umbrellas)
- 6 Decorative enhanced paving

Figure 4-12



Figure 4-13

Plaza Design/Community Activity Area

- 1 Raised planters/decorative blocks to provide seating
- 2 Clear pedestrian connections
- 3 Enhanced landscaping, seating near shade provided by trees
- 4 Water feature
- 5 Shade structure defining space



Pedestrian Alleyway

Alleyways provide utility access and midblock connections for cars and pedestrians within the city fabric. These alleys can also provide **additional public space and create more comfortable pedestrian environments** within the Town Center areas. Public alley design interventions should incorporate outdoor seating, pedestrian amenities, public art, planting areas, and other revitalization strategies to encourage diverse uses.

Standards

- Provide clearly visible pedestrian connections enhanced with decorative paving.
- Use paving patterns to delineate spaces in the public right-of-way and create pedestrian-scaled alleyways.
- Keep alley openings clear and visible to provide sight lines into the space and encourage pedestrian traffic.
- Design spaces to provide a transition, increase transparency, and create interest.
- Incorporate overhead elements such as lighting, shade structures, and public art elements.
- Integrate pedestrian amenities (i.e., site furniture, shading devices, lighting, etc.) into alleyways.
- Plant in large pots, built-in planters, green walls or vertical gardens, where space is limited.
- Use drought tolerant landscaping to enhance and define the alleyway.
- Incorporate permeable surfaces, stormwater planters, and rain gardens to reduce stormwater run-off.
- Incorporate public art within the alley using murals or sculptural elements.
- Clean and repair the alley façade and screen/disguise necessary utility elements, prior to design intervention.
- Avoid obstructing emergency vehicle access with pedestrian and art elements.

Guidelines

- Consider adding additional building entries within alleyways. Existing entries and exits should be improved and/or replaced with more transparent options.
- Continue the primary façade material along the building where it faces the alleys to create cohesion with surrounding streetscapes.
- Allow for flexible, open-areas within the alleyway for street artists/performers or small-scale events.
- Encourage tenants and businesses to incorporate outdoor cafe seating, entry points, or display windows between buildings and pedestrian circulation

Pedestrian Alleyway

- 1 Pedestrian amenities (public art, murals, overhead elements)
- 2 Tenant provided outdoor seating
- 3 Planter boxes
- 4 Clear visible pedestrian connections
- 5 Building entries along alleyway
- 6 Drought-tolerant landscaping
- 7 Decorative paving and landscaping to define alleyway

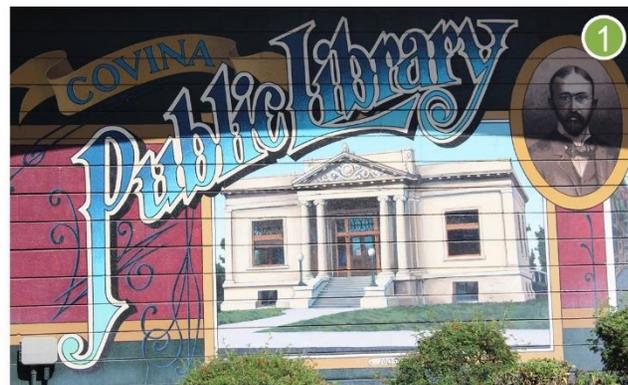


Figure 4-14

local food consumption. Gardens should be provided with adequate space for functional gardening needs.

West Cottage Drive and West College Street Health Corridors

The health corridor or “health walk” will improve access and use of West Cottage Drive and West College Street. A health/fitness themed connection will thematically connect the existing healthcare uses with the Citrus Avenue District. The health corridor should take inspiration from healing gardens, exercise parks, cultural trails, and green streets.

Standards

- Provide a wide planted buffer between vehicle travel lanes and pedestrian and cycle paths. Street trees and other pedestrian-scale elements will encourage more frequent use of the corridor.
- Use decorative paving patterns to break up the pedestrian realm into different speeds of travel, allowing for cyclists, pedestrians, and people with disabilities to move comfortably down the corridor.
- Incorporate universal design features and design for the needs and ease of use for persons with disabilities and universal access.
- Use enhanced crossings to increase pedestrian safety along the corridor. Street crossings must be highly visible and consider the needs of all user groups including children, the elderly, and persons with disabilities.
- Provide clear signage and wayfinding to make travel and navigation easier and to give a “sense of place” to the corridor. Signage should fit into the health theme, provide direction to Citrus Avenue and the Emanate Health Inter-Community Hospital, and incorporate interpretive signage to educate users about the various health benefits of the corridor design.
- Integrate community gardens into the fabric of the corridor, either in an adjacent open space or in smaller interventions along the path. Community garden signage should educate users about healthy eating and encourage

Guidelines

- Consider outdoor exercise equipment dispersed along the corridor. Equipment should be placed in partially or fully screened areas with maximum setback from the street. Interpretive signage can be placed adjacent to the equipment, encouraging healthy lifestyle choices and instructing on use of the equipment.
- Encourage rest and reflection spaces along the corridor. These spaces provide numerous health benefits and may take the form of small plaza or garden spaces off the main path with diverse seating options.
- Recommend using colors, sounds, smells, and textures along the corridor to provide an immersive sensory experience. These can be provided by vibrant planting, small enclosures, water features, pollinator gardens, herb gardens, wind chimes, art, soft groundcovers, vertical planting, and diverse materials. Set these spaces as far back from the street as feasible.
- Encourage small gardens and plazas along the corridor to provide a unique character or message to accommodate users with different needs and interests. This design strategy has been shown to be effective in hospital or healing gardens where many different needs are being met.

West Cottage Drive and West College Street Health Corridors

Figure 4-15



- 1 Rest and reflection spaces/ seating
- 2 Wide planter, lush street trees and landscaping
- 3 Restful seating among landscape areas, outdoor exercise equipment, multi-modal sidewalks, decorative paving, wide and colorfully landscaped buffer
- 4 Community garden
- 5 Multimodal street

Green Infrastructure

Green infrastructure is a powerful tool in the revitalization of public spaces and streetscapes and in the design of new projects. Sustainable stormwater management provides benefits including **city beautification, improved human health and wellness, increased use and occupancy, higher property values, reduced pollution runoff, better water quality and biodiversity, and overall resiliency.**

Standards

- Maximize water infiltration by using natural and/or permeable materials. Consider replacing large paved areas with permeable pavers
- Create areas for local water storage and treatment in strategic locations throughout sites.
- Retain and filter runoff using practices such as permeable pavement, bioretention, tree filters, and underground infiltration.
- Implement permeable pavements within the streetscape or onsite to reduce impervious surfaces and manage runoff from adjacent impervious areas.

Guidelines

- Minimize the impact of stormwater and the extent of retrofits by using site topography, when feasible.
- Use varied strategies for bioretention and filtration that are sensitive to site context.
- Convey runoff across a receiving pervious area to reduce runoff volume. Effective strategies for runoff reduction include vegetated buffers, vegetated conveyance channels and green roofs.
- Improve the visual interest of green infrastructure design by selecting plants with different colors and blooms through all seasons and using

diverse landscape forms such as boulders and channels to create a more welcoming and attractive public realm.

- Select plants that create pollinator habitats.

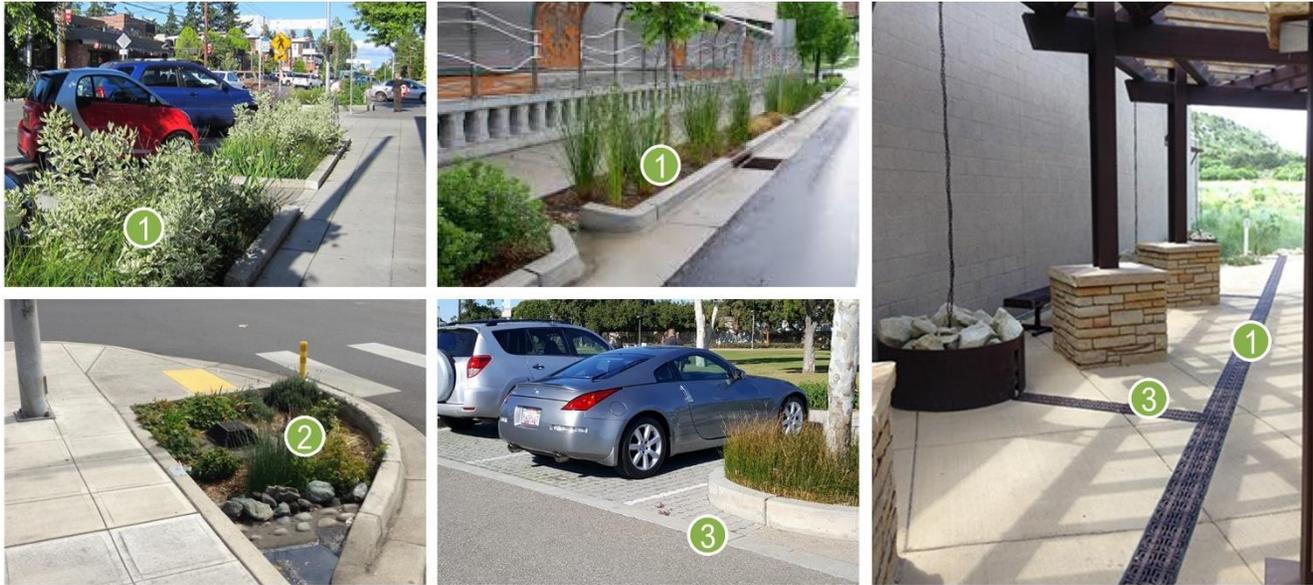
Bioretention basins - landscaped forms collecting runoff from adjacent impervious areas. The collected water is filtered by the soil media and infiltrated to the ground or released to the storm drain. Bioretention basins are applicable to sites with larger landscape areas and can be used to replace water intensive turf areas.

Bioretention planters - similar to bioretention basins but use vertical side walls to maximize stormwater capture within a compact footprint. Bioretention planters are applicable to sites with limited landscaping opportunities and within the streetscape. They are easiest to implement at curb extensions where the planting area is widened to reduce pedestrian crossing distances.

Vegetated channels (or swales) - complimentary to the storm drain system. Vegetated channels or swales can provide additional infiltration and resiliency. Vegetated channels are applicable to sites with long flow paths and may be used in streetscapes with wide right-of-way.

Green Infrastructure

Figure 4-16



- ① Vegetated channel, bio retention planter, pollinator habitat, underground infiltration
- ② Pollinator planting, bio retentor planters
- ③ Permeable pavers

PRIVATE REALM DESIGN STANDARDS AND GUIDELINES

General Building and Site Design

The building and site design standards and guidelines emphasize the orientation of architecture to sidewalks and rights-of-way, increase the visual interest of buildings, relate new and old construction, and emphasize the incorporation and design of elements that provide opportunities for human interaction. Regardless of architectural style, development should exhibit **attention to detail, quality architecture and materials, and a pedestrian-friendly interface** with the sidewalk.

Table 4-1 summarizes key site and building design metrics including building heights, setbacks and stepbacks, open space, and parking requirements. Additional site and building design standards and guidelines follow the table.

Table 4-1: Building Form (Revised: 9-1-2020; 9-20-2022; 4-18-23)

Town Center Districts	Residential	Mixed Use	Commercial	Office	Institutional	Industrial
Maximum Building Height						
Citrus Avenue	n/a	50'	35'	n/a	35'	n/a
Front Street/TOD/F.A.I.R. District	50'	50'	35'	35'	n/a	35'
Civic and Cultural Core	50'	50'	35'	35'	50'	n/a
Downtown West	50'	50'	35'	50'	n/a	n/a
Center Street Neighborhood District	35'	50'	n/a	n/a	n/a	n/a
Ground Floor Height						
	n/a	14' minimum (finished floor to ceiling)	14' minimum (finished floor to ceiling)	14' minimum (finished floor to ceiling)	14' minimum (finished floor to ceiling)	14' minimum (finished floor to ceiling)
Maximum Floor Area Ratio						
All Town Center Districts, see Table 3-1 and Figure 3-2, pages 3-5 through 3-8 for Maximum Floor Area Ratio.						
Maximum Residential Density (du/ac)						
All Town Center Districts, see Table 3-1, pages 3-5 through 3-8 for Maximum Residential Density (du/ac).						
Setbacks						
Front Setback	10' minimum; Front porch may encroach up to 5'	Nonresidential: No minimum, Ground Floor Residential: 10' Minimum	No minimum, 5' maximum if used for landscaping or pedestrian amenities	No minimum, 5' maximum if used for landscaping or pedestrian amenities	No minimum, 5' maximum if used for landscaping or pedestrian amenities	No minimum, 5' maximum if used for landscaping or pedestrian amenities
Side Setback - Adjacent to Nonresidential Use or Zoning District other than R-1	5' minimum	Nonresidential: No minimum Ground Floor Residential: 10' Minimum	No minimum	No minimum	No minimum	No minimum
Side Setback - Adjacent to Existing Residential, School, or Park Use	10' minimum	10' minimum	10' minimum	10' minimum	10' minimum	10' minimum
Side Setback - Adjacent to R-1 Zoning Districts	Where a building is adjacent to an existing R-1 zone along its side and/or rear property lines, the first 2 stories of a structure shall be set back a minimum of 10' from the side and/or rear property line. The building shall step back a minimum of 30' from the adjacent side and/or rear property line for a third story and any story above.					
Rear	10' minimum	Nonresidential: No minimum Ground Floor Residential: 10' Minimum	No requirement	No requirement	No requirement	No requirement
Rear - Adjacent to R-1 or R-2 Zoning Districts	Where a building is adjacent to an existing R-1 zone along its side and/or rear property lines, the first 2 stories of a structure shall be set back a minimum of 10' from the side and/or rear property line. The building shall step back a minimum of 30' from the adjacent side and/or rear property line for a third story and any story above.					
Rear - Adjacent to public alley	5' for building; 3' for parking	5' for building; 3' for parking	No requirement	No requirement	No requirement	No requirement
Rail	Min. 5' setback from rail right-of-way	Min. 5' setback from rail right-of-way	Min. 5' setback from rail right-of-way	Min. 5' setback from rail right-of-way	Min. 5' setback from rail right-of-way	Min. 5' setback from rail right-of-way
Front and Rear Stepbacks						
Front Stepback - Buildings taller than 3 stories	12' average from property line	12' average from property line	n/a	n/a	n/a	n/a
Side/Rear Stepback - Where a building is adjacent to an existing R-1 parcel along its side and/or rear property lines)	First two stories: 10' minimum	First two stories: 10' minimum	n/a	n/a	n/a	n/a
	Third story and above: 30' minimum	Third story: 30' minimum	n/a	n/a	n/a	n/a
Open Space Requirements - Refer to pages 4-34 and 4-35 for private and common open space requirements						
General Requirements	125 s.f. per residential unit	125 s.f. per residential unit	n/a	n/a	n/a	n/a

Table 4-2: Parking Requirements (revised 9-20-2022)

Land-Use Designation	Residential	Mixed Use	Commercial	Office	Institutional	Industrial
Parking Requirements						
General Requirements	Single family, duplex, triplex, attached single-family, townhouses with or without a subdivision: 2 covered spaces per unit	Multi-family development (5 units or more) such as condominiums and apartments: SRO/Studio/1-Bedroom – 1 space per dwelling unit plus 1 guest parking space per each 4 units 2-Bedroom up to 4-Bedroom – 2 spaces per 1 dwelling unit plus 1 guest parking space per each 4 units	1 space for each 200 s.f. of ground floor area	1 space for each 200 s.f. of floor area	1 space for each 200 s.f. of floor area	1 space for each 200 s.f. of floor area
	Multi-family development (5 units or more) such as condominiums and apartments <u>not in</u> Mixed Use/TOD zones: SRO/Studio/1-Bedroom – 1 space per dwelling unit plus 1 guest parking space per each 4 units 2-Bedroom up to 4-Bedroom – 2 spaces per 1 dwelling unit plus 1 guest parking space per each 4 units					
Live/Work	n/a	Live/Work: Up to 2-bedroom per dwelling unit – 1 space per dwelling units plus 1 guest parking space per each 4 units 2+ bedroom unit: 1 guest parking space per unit	n/a	n/a	n/a	n/a
Please reference CMC Section 17.72 for additional parking requirements.						

Setbacks and Stepbacks Standards

Setback dimensions directly affect the scale of development at the street level and, thus, establish the pace and quality of the pedestrian experience. Generally, to enhance the pedestrian experience it is important that commercial uses engage the street, rather than hiding behind parking. On the other hand, residential uses benefit from a setback from the sidewalk to allow for enhancements such as stoops and stairs and increased privacy. All required setback areas shall be used for landscaping and/or private open space.

Key Intersection Maximum 18" Setback Requirement

At key intersections, identified below, the setback of each new corner building or addition to existing corner buildings shall be no more than 18" from the adjoining property line. The key intersection setback is required for the first 100' of building frontage along both intersecting streets. Key intersections are identified as any property located at any corner intersection of the following streets:

- North Citrus Avenue and East Front Street
- North Citrus Avenue and San Bernardino Road
- North Citrus Avenue and Cottage Drive/Italia Street
- North Citrus Avenue and College Street
- Citrus Avenue and Badillo Street
- North 2nd Avenue and East College Street
- North 2nd Avenue and East Italia Street
- North 3rd Avenue and West College Street
- North 3rd Avenue and West Cottage Drive

Commercial Front Setback and Key Intersection Setback Exceptions

- Building Plan Interruptions Exception: Recessed entries, breaks in the building plane, passages to courtyards, and other similar breaks in the street wall are allowed. Up to 30% of the length of a façade and/or front building plane may be interrupted or set back from the maximum setback.
- Outdoor Dining Exception: Where a building has a ground-floor commercial use, a maximum setback exception may be permitted of up to 20' for outdoor cafes.
- Plaza Exception: At any Key Intersection, no maximum setback is required if a large quasi-public plaza space is constructed with direct access and frontage on a public street.

Required Upper-Story Stepback: Public Street

- Include building height modulation to reduce building scale at the street edge for buildings 3 stories or taller fronting on public streets. All stories above the third story shall maintain a 10' stepback from the setback line. The required upper story stepback shall create usable outdoor space, such as a rooftop garden or similar amenity. The Community Development Director/Designee is authorized to allow a reduction through the modification process of the required upper story setback by no more than 5' subject to the following:
 - Offset an additional square foot of public or quasi-public open space (excluding area required for sidewalk dedication or easement) at the street level for each square foot of additional building area proposed within the stepback.
- Locate the public/quasi-public open space along the sidewalk frontage and do not cover open space by buildings.

Front/Side/Rear Stepback Relation to Single Family Residential (R-1, RTCE, RTCW), School, or Park Use

A relational height limit to properties located next to the:

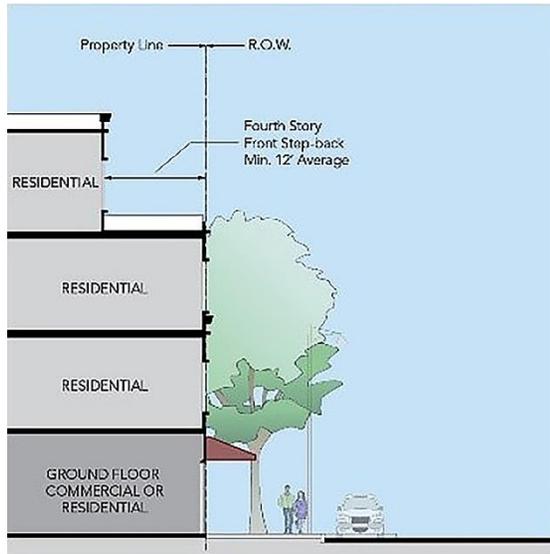
- Single Family Residential (R-1), Residential Town Center East (RTCE), and Residential Town Center West (RTCW) designations; or
- Existing stand-alone residential use, school use, or park use is established to create an appropriate setback and height.

This limit is applied to new development on any parcels that abut another parcel with an existing stand-alone single-family use, school use, or park use, or a R-1, RTCE, or RTCW designation. The stepback requirement varies by use, number of stories, and location as shown in **Table 4-1**.

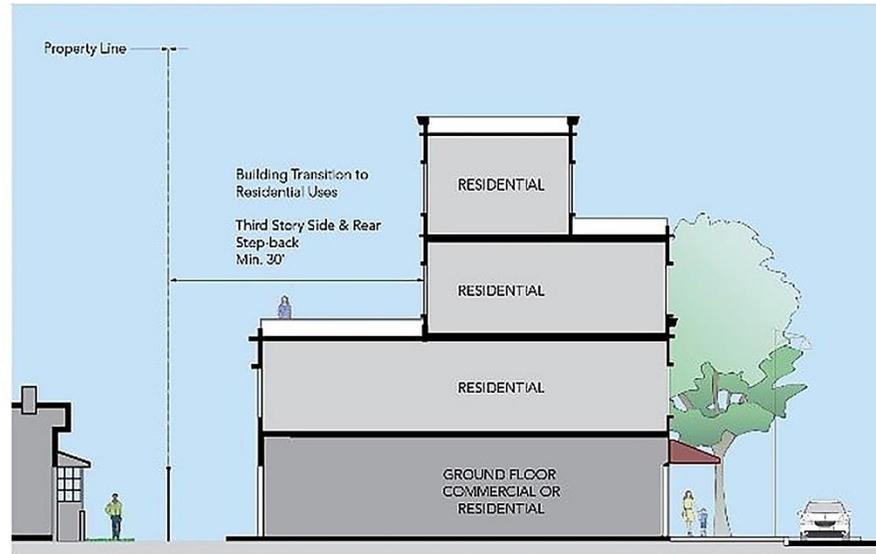
All buildings constructed in the Town Center shall be designed to maximize the privacy of the adjacent homes, backyards, and residential neighborhoods.

Setbacks and Stepbacks Standards

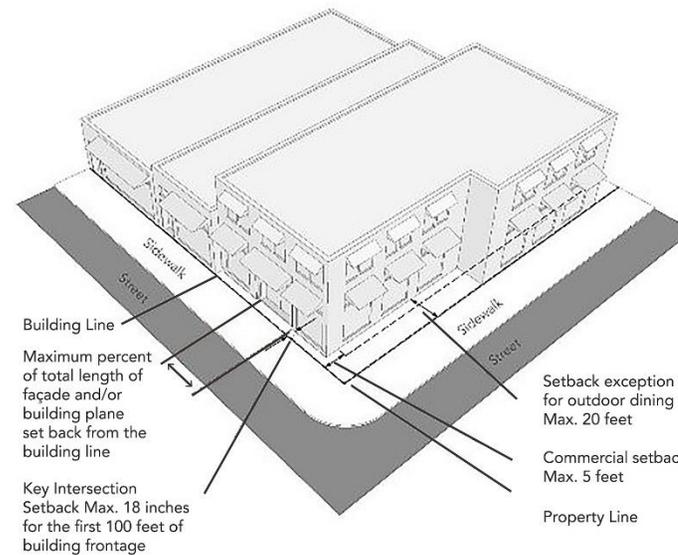
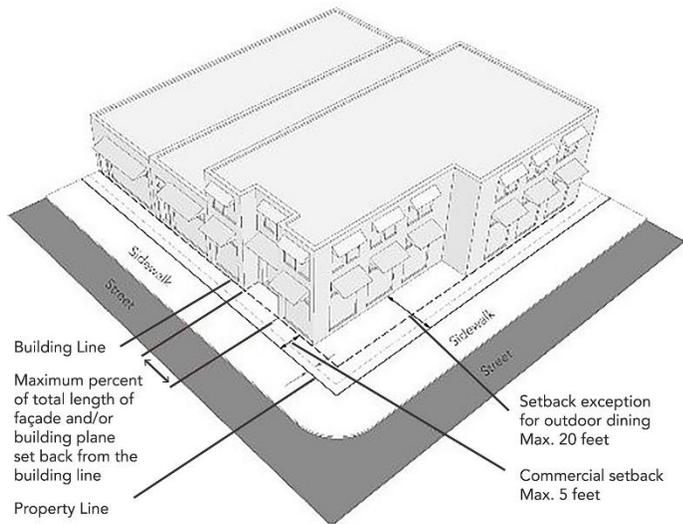
Figure 4-17



Maximum 5-foot commercial setback and key intersection maximum 18-inch setback



Key intersection maximum 18-inch setback and maximum 5-foot commercial setback



Multifamily Residential Open Space

Private Open Space Standards

- Require a minimum private open space of 125 square feet per residential unit with one dimension of the private open space having a minimum length of 8'. This may be accomplished through a private patio or balcony.
- Prohibit any portion of private patio or balcony to be used for the permanent storage of rubbish, junk, clotheslines, or garbage receptacles. "Permanent storage" means the presence for a period of 48 or more consecutive hours on a patio or balcony.
- Require private open space to be open air, not fully enclosed with walls. Private open space cannot be covered by a roof by more than 50% of the area; however, balconies can have up to 100% ceiling coverage.
- Locate residential private open space on the same parcel as the residential units for which it is required.

Common Open Space Standards

- Require a minimum common open space of 275 square feet per residential unit. This may be accomplished through swimming pools, playing courts (tennis, volleyball, etc.), community gardens, libraries, gyms, recreation rooms, roof top decks, etc.
- Require all common open space to be usable. Usable common open space shall be improved to support residents' passive and/or active use. The computation of such common open space shall include no obstructions other than devices and structures designed to enhance its usability, such as swimming pools, changing facilities, fountains, planters, benches, and landscaping.

- Prohibit the requirement for open space to be satisfied through the use of required setbacks, parking areas, driveways, or service areas. Areas designed to accommodate storm water retention may satisfy a portion of the requirement for open space if additional pedestrian amenities are provided near the space in a manner that provides for usability. Compliance with this requirement shall be evaluated by the Community Development Director in the review process.
- Use high-quality porous pavers, porous concrete, or other porous paving materials for all plazas and hardscape. Shade trees shall be provided in all open space. Parks and greenways shall be designed with high-quality benches, lighting, paving, and landscaping.
- Require all common open space to be developed and professionally maintained in accordance with approved landscape and irrigation. All landscaping shall use high-quality materials.
- Permit common open space to be provided on a rooftop where mechanical equipment is located, if the open space is adequately mitigated for noise, odor, vibration, or other impact and is approved by the Community Development Director and Building Official.
- Require a minimum distance of 15' measured horizontally between any swimming pool, jacuzzi, or spa and the nearest point of any balcony, porch, second story patio, sun deck, or other architectural feature of a building or structure with windows, doors, or other openings of sufficient size to permit the passage of persons.
- Require courtyards internal to a project, or enclosed on at least three sides, to have a minimum width of 40' and be landscaped with a ratio of hardscape to planting not exceeding a ratio of one square foot of landscape to one square foot of hardscape. Pools and spas shall be excluded from this ratio.

Nonresidential Use Open Space Standards

Public open spaces are an integral component of large mixed-use or commercial developments. Usable public open space for nonresidential uses or the nonresidential component of mixed-use shall be 5% of the gross parcel area. The nonresidential use open space requirement may be satisfied by the payment of an in-lieu fee, at the discretion of the Community Development Director. Amenities for public plazas, courtyards, alleyways, and pocket parks include trees, lush landscaping, seating, and shade structures. See Plaza Design/Community Activity for other nonresidential open space siting and design guidelines.

Pedestrian Connectivity Standards

Pedestrian walkways or sidewalks shall connect all primary building entrances to one another. Pedestrian walkways shall also connect all onsite public plazas, common areas, parking areas, storage areas, and recreational facilities. Public paseos are strongly encouraged to enhance pedestrian mobility throughout the CTCSP area for all residents, employees, and visitors.

One of the Town Center's key purposes is to enliven local streets and encourage pedestrian activity. Elevated walkways, skywalks, and bridges decrease the likelihood of walking at street level. As such, elevated walkways, skyways, and bridges between buildings are allowed under the following conditions:

- Permit a maximum of one elevated walkway, skyway, or bridge per building.
- Allow elevated walkways, skyways, or bridges if they do/are not:
 - cross over a street, public park, or public open space;
 - connect buildings on Citrus Avenue, Badillo Street, nor San Bernardino Road; and
 - adjacent to single-family residential uses.

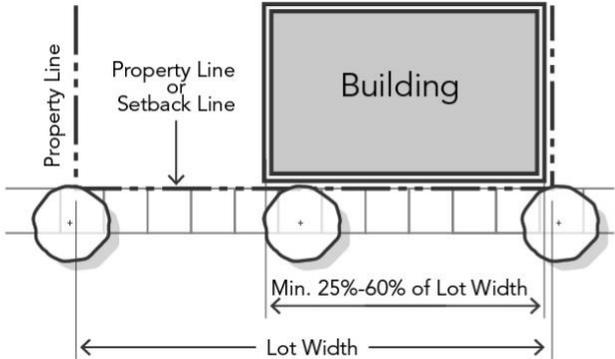
Mixed Use and Nonresidential Site Design Standards

- Locate storefronts and usable nonresidential space along the buildings' required ground floor street frontages and require a minimum usable depth of 40' along 60% of the length of the building frontage. In no case, shall the storefront or usable nonresidential space be less than 20' in depth. At a corner lot where storefronts and nonresidential uses are required, storefronts and nonresidential spaces shall turn and wrap around the corner for a minimum length of 20'.
- Allow only one vehicular access point for parcels of less than 450' in length along Citrus Avenue, East Front Street, Badillo Street, and San Bernardino Street. For all other lot frontages, a maximum of one vehicular access point for each 300' of street frontage is permitted.
- Place building on the property line (nonresidential or mixed use) or setback line (residential use) for 60% of the lot width.
- Consider a second floor's building overhang (drip line) as building frontage if the outdoor ground floor faces Citrus Avenue or East Front Street and the outdoor ground floor contains a restaurant dining space or pedestrian plaza. The outdoor dining space or the outdoor pedestrian plaza must be designed for such use and include both decorative hardscape and landscape.

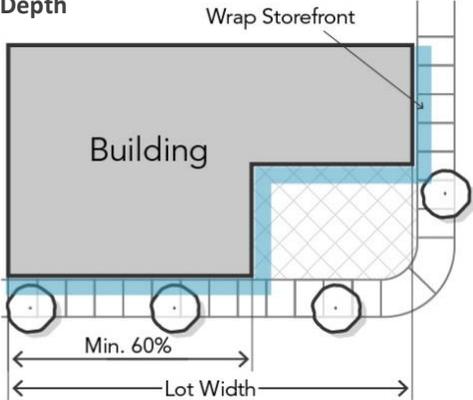
Mixed Use and Nonresidential Site Design Standards

Figure 4-18

Building Placement



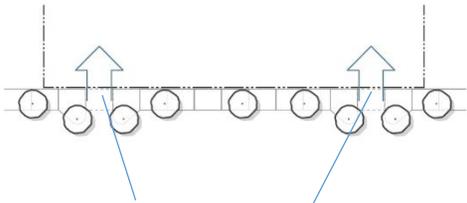
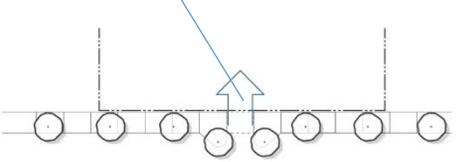
Usable Depth



Curb Cuts

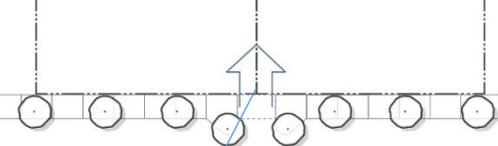
1. Minimize the number of entrances and exits to parking areas in order to minimize conflicts with pedestrians and reduce congestion at street intersection.
2. Share parking and or share access (entrances and exits) between adjacent parcels/properties to the extent feasible.
3. Design vehicular circulation to allow through movement between adjacent parcels/properties.
4. Use covenants and deeds to ensure shared access.

Curb Cut/Access Drive



Curb Cut/Access Drive

Shared Curb Cut/Access Drive



New Building Façades

Appropriate building facade design ensures the appropriate scale and character of buildings in the Town Center. Finely detailed and articulated building facades create a rich character and human scale. The following standards and guidelines apply for all new residential and nonresidential development and upgrades to existing structures.

Standards

- Incorporate a 360-degree building design philosophy, understanding that all the built environment should be well designed, not just that visible from public areas. Similar and complementary massing materials and details must be incorporated throughout the facades.
- Avoid blank walls and provide the highest level of articulation on all walls visible from adjacent streets and public spaces.
- Apply materials in a consistent manner to all facades of the project. Building materials, colors, fenestration, scale, and massing are to be compatible with the overall architectural style.
- Create visual interest in building facades and break up the mass of large-scale buildings with articulation in form, architectural details, and changes in materials and colors.
 - Use articulation in form, including changes in wall planes, upper-story building stepbacks and/or projecting or recessed elements.
 - Modulate the façade planes. The wall plane of street-facing façades shall be modulated a minimum of 18" perpendicular to the plane at least once every 60', as measured along the property line.
 - Incorporate architectural elements and details on all walls, such as adding notches, grouping windows, loggias and dormers, adding canopies, wing walls, trellis features, arcades, and colonnades; varying cornices and rooflines.
- Vary materials, colors, and textures to enhance key components of a building's façade (i.e., window trims, entries, projecting elements, etc.).
- Create virtual boundaries by a varying/changing level, materials, textures, and color.
- Place murals, espaliers/trellises and/or vines on large wall expanses.
- Create contextual fit with surrounding residential uses and/or the neighborhood's historic character within the RTCE and RTCW designations.
- Create a contextual fit with articulated building form including strong massing and horizontal division (base, middle, top) in all other designations.
- Capitalize and further the historic look and feel of the district for buildings within the Historic Core designation.
- Break roof lines at intervals no greater than 50'-long by changes in height or roof form.
- Incorporate projections, overhangs, recesses and banding.
- Roof parapet or separate roof screen wall must be architecturally integrated to building design for screening of roof equipment and projections.
- Design ground floor to be high-quality, pedestrian-oriented, and sensitive to the use. Residential units' ground floor design must be significantly different than the design for nonresidential. Storefront configurations and details providing a sense of human scale, variety, and interest within the overall context of buildings' bays and groupings of bays are required. Such details include but are not limited to recessed entries at storefronts, recessed storefronts, display windows, projecting bays, integral awning, utilization of true dividing mullions, transoms over entries, and integral signs and sign boards.

The minimum height of nonresidential ground floor spaces shall be 12'. The minimum height of residential ground floor spaces shall be 10'. This height shall be measured from the interior floor of the first story to the ceiling of the first floor. If there is no second story, the height shall be measured to the top of roof (including pitch).

Guidelines

- Preservation and adaptive reuse of historic buildings is strongly encouraged and prioritized, regardless of location.
- Activate upper-story setback areas with balconies or roof gardens.
- Nonresidential uses should use building setbacks and arcaded spaces as an extension of the sidewalk to provide adequate space for pedestrian movement and activity. This space can be used for outdoor seating, street furniture, landscaping, public art, and outdoor dining that can enliven the streetscape.
- Architecturally compatible lighting and fixtures should be incorporated that are complementary to the intended style or theme.
- Empty and/or vacant storefronts shall have decorative murals or coverings of the storefront windows until unit is leased.

Existing Building Façade Enhancements

In conjunction with, or in advance of, more significant redevelopment projects, many modest improvements can be made to existing properties and buildings to contribute to the Town Center's sense of place and revitalization. Façade renovations must involve the general upgrading of a building's external appearance.

Modifications to existing buildings including facade enhancements, remodels, and building additions shall comply with all design standards listed in this Specific Plan to the extent feasible. Guidelines that require a site redesign, such as building orientation to the street, shall not apply.

Where building orientation precludes conformance with design guidelines, the following additional guidelines apply:

- Pedestrian Access: A system of pedestrian walkways shall connect all buildings on a site to each other, to onsite automobile and bicycle parking areas, and to any onsite open space areas and pedestrian amenities including the public sidewalk. An onsite walkway shall connect the primary building entry or entries to a public sidewalk on each street frontage. Such walkway shall be the shortest practical distance between the primary entry and sidewalk, generally no more than 125% of the straight-line distance.
- Building Identity: Façade enhancements shall help establish building or business identity and incorporate colors and materials that support the overall design concept.
- Architectural Style: Minor changes to a building façade should be consistent with the building's original architectural style (for example, window and door replacement and repair). For major changes that encompass an entire facade improvement, or if completing a comprehensive building redesign, the design should be consistent with principles of composition that are typically associated with that style as evidenced in precedents, relevant examples, and the design guidelines of this Chapter.

Building Façades

Guidelines

- 1 Visual interest in building façades
- 2 Modulated roofline
- 3 Pedestrian realm articulation
- 4 Enhanced pedestrian realm
- 5 No blank walls
- 6 Large buildings with form (modulation and articulation)
- 7 Change of materials and colors



Figure 4-19

Building Façades

- 1 Large windows
- 2 Enhanced design of original building
- 3 Street-facing patio

Figure 4-20

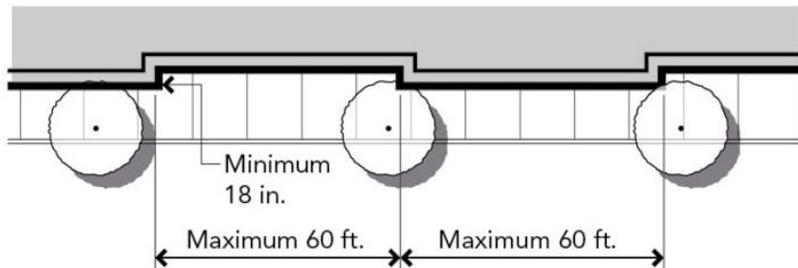


Building Façades

Figure 4-21



Building Height



Facade Articulation



- ① DOORS
- ② WINDOWS
- ③ FACADE TREATMENTS
- ④ DECORATIVE BUILDING MATERIALS
- ⑤ BLANK WALL

Ground Floor Design

Ground Floor Design

Stand-Alone Multifamily Residential and Horizontal Mixed-Use (Residential Component)

In addition to the other guidelines outlined in this Chapter, the following stand-alone multifamily residential and horizontal mixed use (residential component) design standards and guidelines apply to new construction, additions, and existing structures' alterations. **The transition from public sidewalk to private space is especially important** when residential uses are located at the ground floor. These design standards and guidelines apply to areas where ground-floor residential uses are permitted.

Standards

- Provide transitional spaces in the form of stoops, overhangs, and porches between public areas and entrances for new multifamily residential buildings.
- Provide decorative lighting at entries.
- Incorporate staircases into the overall architectural massing of the building when exterior staircases are used.
- Prohibit metal staircases unless used in an industrial building's adaptive reuse.

Guidelines

- Allow a distinctive entry to be established for each, individual exterior entry. Architectural elements should be consistent with the development's architectural style.
- Encourage porches with a minimum 5' dimension.
- Design residential project to avoid large box-like forms with continuous un-relieved surfaces.
- Minimize the bulk of the building length or design buildings with:
 - Vertical and/or horizontal setbacks and stepbacks
 - Changes in roof form and height
 - Breaking up building into smaller massing

Ground Floor Design

- 1 Architectural projections
- 2 Decorative head and sill
- 3 Recessed windows and doors
- 4 Offset architectural features
- 5 Individual unit entries
- 6 Decorative entries
- 7 Porches and landscape frontage

Figure 4-22



Industrial, Stand-Alone Commercial, Horizontal Mixed-Use (Commercial Component)

In addition to the other standards and guidelines outlined in this Chapter, the following industrial, stand-alone commercial, and horizontal mixed-use (commercial component) design standards and guidelines apply to new construction, additions, and alterations. Ground-floor **commercial and industrial spaces should be inviting to the pedestrian and draw visitors into the space.**

Standards

- Design nonresidential use's ground-floor space to orient tenant spaces to the street and maximize storefronts and entries along the sidewalks to sustain street-level interest and promote pedestrian activity.
- Design each street-level tenant space that fronts a public street to provide the primary entrance/access from that street.
- Require each street-level tenant space that does not front a public street to provide the primary access/entrance from a pedestrian paseo, courtyard, or plaza that is connected to the public street.
- Direct wall openings, such as storefront windows and doors, to comprise at least 50% of a building's street-level facade. Clear glass for wall openings, i.e., doors and windows, shall be used along all street-level facades for maximum transparency, especially in conjunction with retail uses. Dark tinted, reflective, or opaque glazing is not permitted for any required wall opening along street level facades.

Guidelines

- Locate activity uses on the ground floor and provide continuous storefront windows and highly visible articulated entries. Pedestrian-oriented features include:
 - Wider sidewalks
 - Building entrances and façade articulation
 - Outdoor cafes
 - Plazas or other public/private spaces that provide area for activity
- Recommend storefront configurations and details that provide a sense of human scale, variety, and interest within the overall context of the buildings.
- Encourage ground-floor nonresidential space to wrap the corner onto the intersecting streets.
- Support the inclusion of Wi-Fi hotspots to attract pedestrians and encourage gathering at specified locations.
- Promote the use of natural light through use of large window expanses, clearstories, skylights, etc. to enhance working spaces and reduce lighting energy consumption.

Industrial, Stand-Alone Commercial, Horizontal Mixed-Use (Commercial Component)

Figure 4-23

- 1 Individual entries
- 2 Large windows to allow natural light
- 3 Pedestrian orientation
 - Wider sidewalks
 - Outdoor cafes
 - Plazas
- 4 Multiple storefront entries
- 5 Human scale
- 6 Window/doors 50% of frontage



Vertical Mixed-Use Development

In addition to the other standards and guidelines outlined in this Chapter, the following vertical mixed-use design standards and guidelines apply to new construction, additions, and alterations. Mixed-use development plays a vital role in creating **neighborhoods where people can walk between home, work, shopping, and recreation**. The primary design issue related to mixed-use projects is the need to successfully balance the requirements of residential uses (such as the need for privacy and security) with the needs of nonresidential uses for access, visibility, parking, loading, and the possibility of extended hours of operation. Storefronts should be characterized by continuous building frontages, adjacent to, and with awnings over the footpath. Mixed-use development should result in the formation of a focal point for retail, office, entertainment, recreation, and community-related activities for the immediate area.

Standards

- Differentiate residential and nonresidential vehicular and pedestrian access with paving material, color, landscape buffers, etc.
- Provide distinctive signs for identification and guidance, appropriate to each use.
- Focus lighting, including reflected light, so that residential areas receive minimum glare.
- Add noise-attenuating protection between floors and between units for noise-sensitive uses and to provide privacy for residential areas.
- Locate accessory structures to contribute to a visual and functional separation.
- Design public spaces to be clearly recognizable as “public” (e.g., a plaza within view of a street or other public space) and publicly accessible. Design private spaces to be clearly recognizable as “private” using gates and signs.

- Screen private areas from public view using landscaping, walls, fences, and changes in elevation.
- Design or locate public or semi-public spaces to encourage year-round use by providing areas that have awnings, sunshades, and/or landscaping that can provide shelter from the elements.

Guidelines

- Orient uses to facilitate proper transitions to surrounding buildings and adjacent uses.
- Use landscape features to highlight individual uses.
- Encourage the overall design palette to include building materials and textures that define each use as part of an overall theme.
- Encourage areas to be designed to encourage informal meetings and social interaction with other people.
- Promote overall themes and visual connections between spaces and uses to be provided within the development, including pedestrian linkages throughout the development and to adjacent land uses.
- Support the inclusion of Wi-Fi hotspots to attract pedestrians and encourage gathering at specified locations.

Vertical Mixed-Use Development

Figure 4-24

- 1 Pedestrian-oriented
 - Wide sidewalks
 - Articulated facades
- 2 Façade modulation
- 3 Place making elements
 - Architectural element at corner
 - Ample public space
- 4 Varied window head and sills
- 5 Differentiation between residential and non-residential uses
- 6 Recessed private open space
- 7 Building materials and color set design theme
- 8 Large windows and doors



Vertical Mixed-Use Development

- 1 Articulated façade with private space
- 2 Set back and/or step back massing
- 3 Distinctive materials for private space
- 4 Façade articulation with balconies
- 5 Landscaping separates private and public space
- 6 Screening separates private space from public



Figure 4-25

F.A.I.R. Incubator Space Development

In addition to the other standards and guidelines outlined in this Chapter, the following incubator space design standards and guidelines apply to new construction, additions, and alterations. Incubator development space **provides unique opportunities** by creating spaces that benefit small business, creative industries, innovative thinking, and advanced technologies. Incubator development space **fosters entrepreneurship and creativity** by encouraging commercial and industrial development that **promotes new and established business and helps sustain economic growth**.

Standards

- Build upon existing context through use of similar forms, heights, proportions, building materials and details observed in the surrounding area.
- Conserve and retain the character defining features of existing buildings:
 - Minimize the removal of older components, materials, and details;
 - Repair such features rather than replace them, to the extent feasible; and
 - Encourage additions that complement the existing character.

- Build on the quality industrial character of architecture currently in the district.
- Convey a sense of old and new through conservation of existing materials and details and selection of new building components that complement existing conditions without mimicking an older architectural character.
- Incorporate small, medium, and large scales through design of massing and a variety of components, proportions, and details.
- Encourage the use of roll-up doors and sliding walls at street frontages to add visual interest at the pedestrian level and enhance indoor-outdoor interactions.
- Orient secondary entries to alleys to create a sense of arrival and increase pedestrian presence in alleys.
- Support the inclusion of Wi-Fi hotspots to attract pedestrians and encourage gathering at specified locations.

Guidelines

F.A.I.R. Incubator Space Development

Figure 4-26

- 1 New additions complement and/or retain character of existing structure
- 2 Small, medium, and large-scale elements
- 3 Conservation of existing materials and details, large windows allow natural light



F.A.I.R. Incubator Space Development

- 1 New additions complement and/or retain character of existing structure
- 2 Small, medium and large-scale elements
- 3 Conservation of existing materials and details, large windows allow natural light



Figure 4-27

Entries, Doors, and Windows

Entryways are an important design feature in buildings as they **guide guests to the interior and provide opportunities for architectural definition**. Similarly, the use and location of windows and secondary entrances are a key component of building design.

Standards

- Incorporate one or more of the following in entrance design:
 - Place art or decorative detailing at the entry
 - Place a projecting element above the entrance or a portico or formal porch projecting from or set into the surface
 - Design a change of material or detailing
 - Implement architectural elements such as flanked columns or decorative fixtures
 - Recess doors, archways, or cased openings
 - Change the roofline or add a break in the surface to the subject wall
- Locate pedestrian-scale windows at street level to maximize interior daylighting and views.
- Incorporate glazing (windows) and openings to provide light to adjacent spaces, rooms, and uses on all street-facing building facades. Windows and openings facing streets shall constitute a minimum of the following percentage of street-facing building faces:
 - 50% at commercial ground floor uses
 - 30% at commercial upper floor uses
 - 30% at industrial ground floor uses
 - 20% at industrial upper floor uses
 - 40% at multifamily residential ground floor uses
 - 30% at multifamily residential upper floor uses

- Require the use of clear glass for nonresidential buildings at the ground floor. After installation, clear glass windows at the ground floors should be laser treated so as to become opaque or to be blocked so as to prevent visibility of the ground floor interior from the sidewalk. Signs, posters, displays, furniture, etc. shall not inhibit transparency into a nonresidential use.
- Prohibit the use of reflective or mirrored glass.
- Prohibit the use of metal bars, gates, or screens.
- Create a sense of entry at a pedestrian scale through well-designed nonresidential storefronts including windows, doors, wall composition, colors, and materials.
- Require uses to be vacant 30 or more days to activate ground floor windows and doors with translucent decorative art images.

Guidelines

- Encourage windows and doors within a single facade to have the same style, height, and scale with the building elevation.
- Encourage windows and doors to include accent elements such as shutters, moldings, and divided lights.



Figure 4-28

Entries, Doors, and Windows

- 1 Change of materials at windows and doors
- 2 Projecting elements over doors and windows
- 3 Defined sense of entry
- 4 Recessed doors and windows
- 5 Pedestrian-oriented windows

Figure 4-29



Entries, Doors, and Windows

Figure 4-30

- 1 Projecting elements
- 2 Decorative detailing
- 3 Uniform window size
- 4 Large, clean windows
- 5 Mural and/or graphics to mask vacant storefront windows



Architectural Projections Standards (revised 9-20-2022)

Height Projections

- Permit architectural features for multifamily, commercial, industrial, institutional, and mixed-use buildings to project up to a maximum of 10' above the maximum allowed height, subject to Design Review. Architectural features include rooflines (balustrades, pediments, statuary, dormer windows, cross gables), masts, towers, turrets, eaves, rafters, non-habitable building façade, skylights or roof windows, utilities, utility penthouses, roof top decks, roof top trellis, roof top patio cover, and solar panels.
- Prohibit height exception from exceeding the allowable number of stories permitted pursuant to regulations in this Specific Plan.
- Refer to CMC Section 17.26 for single family residential architectural projections.

Setback Projections

- Allow lobbies, porches, stoops, and other entry related architectural features to extend up to 4' into the required front setback area for multifamily, commercial, institutional, industrial, and mixed-use buildings. Upper-floor balconies may extend up to 2' into the setback area; up to 3' may be considered depending on design and Community Development Director approval.
- Permit upper-floor balconies, where an upper-story stepback is required, to extend up to 2' into the required stepback; up to 3' may be considered or up to 5' for rear setbacks depending on design and Community Development Director approval.
- Refer to CMC Section 17.26 for single family residential setback projections.

Materials and Finishes Guidelines

The choice of materials is one of the most important contributors to defining the character of a building. Materials should be of high quality and detail to provide visual interest and should **suggest durability and permanence** to last into the future.

- Encourage materials and finishes to be used consistently and be appropriate to the architectural style of the building.
- Promote materials and finishes for new buildings in the RTCE and RTCW designations to be consistent and contextual with the neighborhood.
- Encourage the use of contrasting colors for trims, windows, doors, and other key architectural elements. Colors used should be a coordinated palette of complimentary colors rather than a patchwork of compete colors.
- Recommend high-quality, long-lasting materials be used for exterior windowsills and trims that are consistent with the overall architectural style of the building.
- Recommend finish materials terminate at changes in the wall plane and not on the same plane.
- Encourage roof materials to complement the materials and colors of the facades and provide texture or relief.
- Integrate rain gutters and downspouts into the facade. At a minimum, their color should blend with adjacent surfaces.
- Derive trellises, architectural canopies, balconies, and other such design elements' materials, colors, and form from the building's architecture.
- Darker colors may be used for trim.
- Do not use fluorescent or neon colors.

Architectural Projections Standards | Materials and Finishes Guidelines

Figure 4-31

- 1 Header and sill color variation
- 2 Roofline variation
- 3 Roof materials compliment façade

Projections, accent colors, materials change in wall plane, contrasting colors for trims, windows, building design and materials appropriate for all districts other than F.A.I.R.



Architectural Projections Standards | Materials and Finishes Guidelines

Figure 4-32

- 1 Materials, finishes, and colors appropriate for architecture
- 2 Architectural projection above maximum height
- 3 Roof materials provide texture and relief
- 4 Contrasting trim and materials and colors
- 5 Building design and materials appropriate for F.A.I.R. district



Lighting (revised 9-20-2022)

Exterior building lighting is important both for providing visibility and safety as well as creating ambiance. Lighting can be used to **enhance architectural details and landscape features**, and to **illuminate sidewalks, pedestrian paths, and plazas**.

Standards

- Design light fixtures and poles to be architecturally compatible with the building's architectural style.
- Require building entrances to be well-lighted with appropriately scaled light fixtures.
- Prohibit fixtures from casting light directly into adjacent residential properties; a translucent or optical lens diffuser globe or shield is recommended.
- Design light fixtures so that the light goes exactly where it is intended. Take special care to include louvers, glare shields, or barn doors to the front of floodlight fixtures to prevent light pollution.
- Require light levels to be appropriate for illumination intended. Avoid lighting that becomes a "beacon".
- Incorporate lighting into architectural features such as doors, window openings, detail cornices, columns, and arcades to create texture and form unique to the building.
- Require all fixtures and wiring to be well hidden in the architectural details so that the lighting has a minimal impact during the day.
- Mount lighting fixtures in strategic locations to facilitate maintenance.
- Maximum height for light fixtures shall be 20 feet, measured from the finished surface to the top of the light fixtures.

Guidelines

- Encourage color and finish of lighting metalwork, when used, to harmonize with building metalwork, if applicable.
- Enhance the front building facades during twilight and night-time hours with architectural lighting.
- Recommend "close-in" lighting of stone and brick building facades so that light grazes across the surface and calls attention to its textural quality by creating shadows.
- Emphasize a base, middle, and top of the building, thus, allowing the building to be viewed from several different vantage points, both near and far from the structure, without looking unnatural.
- Avoid situations where a building facade is washed with bright light from a distant location. This approach "flattens" out the building's texture and causes unnecessary glare to the nighttime users.
- Recommend architectural and landscape lighting.

Lighting

- 1 Appropriate light levels
- 2 Architecturally compatible
- 3 Color coordinates with building materials
- 4 Lighting design features

Figure 4-33



Signs (revised 9-20-2022; 4-18-23)

The design of building signs is important for **communicating information, reinforcing the architecture** of the building, and contributing to the Town Center’s **overall character**.

The following sign standards and guidelines are to encourage high quality and unique signs, to complement and enhance the appearance of the buildings and the integrity of the architectural design while providing appropriate identification for businesses. In cases not covered by the sign standards and guidelines, the CMC Section 17.74 and 17.75 shall govern.

Standards

- Signs are for business identification only. The name of the business including any registered trademark is allowed. Extraneous information such as phone numbers, hours of operation and list of products or services are not allowed.
- Require signs to be subtle rather than dominate a space.
- Allow sign shapes, type styles, and color combinations to complement building styles and reflect the business they represent.
- Require signs to be wall-mounted or suspended from awnings above the sidewalk. Locate building-mounted signs on wall areas or architectural features specifically designed for signs, such as recessed wall areas, towers, turrets, or parapets.
- Prohibit signs painted directly onto building walls unless historically accurate for the particular historic structure, unless historically accurate for the particular historic structure or contribute to the significance of the historic area, subject to the Director review and approval.
- A Sign Program shall be required for multi-tenant, non-residential buildings and multi-buildings for multiple-family residential or mixed-use complex.
- Building address display for multi-story buildings or building with multiple units: minimum 8” and maximum 12”, and address affix below the roof parapet in a position that is visible for location identification. Where required by building official or fire code official, building address shall be provided in additional locations to facilitate emergency response.
- Building address display above the front door entry: maximum 4”.
- The use of cannister type box signs with translucent backlit panel are not allowed.
- Wall Signs. Maximum 1 wall sign per building frontage. A corner unit with a side street building frontage may have 1 additional wall sign. Maximum sign area for each wall sign shall not exceed 15% of the area of first story height of the building. Maximum letter height shall not exceed 24”.
- Wall sign shall not display above the second floor window sill for Historic Core District, unless historically accurate for the particular historic structure or contribute to the significance of the historic area, subject to the Director review and approval.
- Blade Signs. Blade signs must be constructed of a uniform finished with high quality materials to insure longevity. Business name or logo to be flat or dimensional. The lowest point of a blade sign shall be a minimum of 8’ above grade.
- Awning Signs. For angled metal awning, sign panel must be installed vertically below the awning. For horizontal metal awning, sign panel may be mounted on top, below or to the face of the horizontal metal awning.
- Alley Signs. An alley sign shall not exceed 6% of the area of first story height of the building.
- Window Signs. 2 per building occupancy. Maximum 20% of window space in which they are displayed. Each sign shall be of a permanent nature. No second floor window signs.
- Temporary Signs. Comply with CMC Section 17.75.
- Monument Signs. 1 sign for property with 45’ or more street frontage. Maximum sign area: 20 square feet. Maximum sign height: 6’. Sign must setback 5’ from property line.
- Display of City of Covina approved historic citrus label (decal) on windows is exempt from window signs.

- Freestanding Signs. 1 sign for property with 150' or more street frontage. Maximum sign area: 75 square feet. Maximum sign height: 20'. Sign must setback 10' from property line.

Signs

- ① Reflects business and/or use
- ② Perpendicular to wall
- ③ Individually lettered
- ④ Incorporated into building
- ⑤ Subtle design complements architecture or architectural feature



Guidelines

- Encourage signs that reflect a crafted, high-quality, detailed design approach.
- Advocate signs to reflect the uses they represent in creative and fun, as well as functional, ways.
- Recommend facade signs to be individually lettered.
- The use of backlit individually cut letters is encouraged. Direct or indirect lighting methods are allowed.
- Encourage blade signs to be hung perpendicular to the site wall.

Figure 4-34

Awnings

Awnings provide **visual interest and pedestrian scale at the street level**. While awnings may not be appropriate along every facade, in combination with tree canopies, awnings can provide shade and shelter for pedestrians. Awnings should be encouraged as building facade enhancements wherever appropriate.

Standards

- Require awnings to be in scale with the building and located so as not to inhibit pedestrian movement or views.
- Permit awnings on the first and second stories; above the second story, structured trellis shall be used. However, structured trellis can be used in lieu of awnings on the first and second stories.
- Place awnings relative to major architectural elements of the façade. Avoid covering transom windows or architectural elements such as belt courses, decorative trim, or other notable architectural facade elements.
- Prohibit awnings from covering building piers.
- Require all awnings to be maintained and cleaned on regular basis and replaced when damaged or discolored, to ensure a consistent desirable presence.
- Mandate awnings be constructed of durable materials such as canvas or metal.
- Awnings and other projections into public right-of-way shall require an annual encroachment permit.

Guidelines

- Encourage awnings, overhangs, and arcades where pedestrians can walk and shop, providing overhead protection and highlighting entrances.
- Promote awnings to be designed to be decorative, complementary to the overall facade design, and effective for weather and sun protection.

Awnings

- 1 Durable metal material
- 2 Pedestrian-scale
- 3 Weather protection
- 4 Complement façade design
- 5 Highlight entrance



Figure 4-35

Accessory Structures Standards (revised 9-20-2022)

- Screen all utilities from public view by incorporating the utilities or accessory structures into the overall architecture. Development must comply with all utility company requirements for access, service, and ownership.
- Locate mailboxes in alcoves away from streetscape.
- Design trash enclosures with similar finishes, materials, and details of the primary building.
- Separate trash enclosures from adjacent parking with a minimum 6'-wide planter.
- Require 5' setback from rear, interior side, and street side yards.
- Trash enclosure areas not permitted within front yard.
- Locate enclosings away from view of the primary entrance drives or streets.
- Size trash enclosures for individual units and/or common facilities to accommodate separate bins for recyclable trash and regular trash, as well as green/yard waste (when appropriate).
- Refer to CMC Section 17.26 for accessory structures associated with single family residential uses.

- 1 Overhang/trellis
- 2 Complements architecture with similar materials and finish



Building/Structure Enhancements at Key Intersections

Some Town Center intersections present opportunities to create a **strong and positive visual impact and provide great landmark potential** for the community. In addition to the other standards and guidelines outlined in this Chapter, the following design standards and guidelines apply to new construction and additions, as well as alterations to existing structures, located at key intersections. These intersections are:

- North Citrus Avenue and East Front Street
- North Citrus Avenue and San Bernardino Road
- North Citrus Avenue and West Cottage Drive/East Italia Street
- North Citrus Avenue and College Street
- Citrus Avenue and Badillo Street
- North 2nd Avenue and East College Street
- North 2nd Avenue and East Italia Street
- North 3rd Avenue and West College Street
- North 3rd Avenue and West Cottage Drive

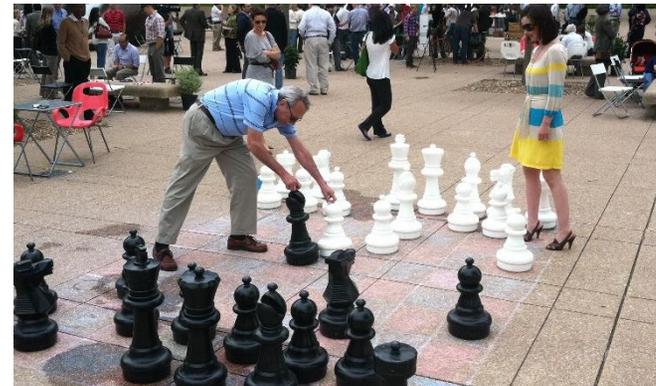
Standards

- Require pedestrian amenities (such as a shaded plaza area, generous walkways, and clear and open pedestrian links to the corners) be incorporated to help create a sense of place.
- Prohibit new surface parking spaces and lots on corner sites of key intersections.
- Provide visual themes at all four corners with paving, wall forms, and landscape materials.
- Incorporate landscape material designs into the private frontage area, where plantings are appropriate.
- Design and construct functional plazas with a minimum area of 600 square feet with the minimum length of smallest dimension of the plaza being no less than 25’.

Guidelines

- Encourage landmark qualities by installing public monumentation, themes, public signs, and art.
- Promote special architectural elements to be incorporated on buildings, such as articulated display windows and entrances or a taller, more prominent roof forms or elements.
- Program placemaking activities and events and design identifiable elements and amenities that activate the plaza space.

Figure 4-37



Building/Structure Enhancements at Key Intersections

Figure 4-38

- 1 Taller entrance
- 2 Architectural element reflecting corner location
- 3 Landmark sign/art
- 4 Prominent architectural feature
- 5 Outdoor plaza space



Parking

The following design guidelines apply to all parking areas within the Town Center. Parking areas should be designed for functional vehicular access and circulation, with enhanced pedestrian connections that are aesthetically pleasing and shaded.

Bicycle Parking Standards

- Nonresidential Uses - Permanently anchored bicycle parking shall be provided for 5% of the required vehicular parking for the use, with a minimum of one 2-bike capacity rack, as defined in California Green Building Code Section 5.106.4.1. Bicycle racks shall be located within 200' of a visitor entrance and readily visible to passers-by. See Section 5.106.4.1.1 of the California Green Building Code.
- Single Family Residential Uses – Refer to CMC Section 17.26.
- Multifamily Residential Uses - For all residential buildings containing more than three dwelling units, long-term bicycle parking shall be provided at a rate of one per dwelling unit. Developments that include individually accessed private garages for each unit shall not be required to provide separate long-term bicycle parking. Acceptable examples of long-term bicycle parking include bicycle lockers, bicycle rooms, and bicycle cages. Long-term bicycle parking spaces shall be a minimum of 18" wide.
- Encourage electric bicycle charging stations.

Off-Street Vehicle Parking Standard

F.A.I.R. parking requirements may be addressed with an in-lieu fee with the approval of the Community Development Director/Designee. Comply with the off-street parking regulations of CMC Section 17.72 unless otherwise specified here. Town Center off-street parking regulations are designed to allow flexibility for uses appropriate to each specific district environment.

Off-Street Vehicle Parking Guidelines

- Encourage the consolidation of off-street parking areas, lots, and garages.
- Recommend developers work with adjacent property owners to create shared parking facilities.
- Encourage the payment of in-lieu parking fees, at the discretion of the Community Development Director, for the construction and operation of shared parking facilities.
- Encourage electric vehicle (EV) charging stations.

Transit Adjacent Reduced Parking Requirements

A parking reduction may be granted for nonresidential and residential uses proposed adjacent to local or regional mass transit lines or routes. The reduction may be granted with the approval of the Community Development Director based upon:

- findings from parking demand studies provided by the applicant, prepared by an independent licensed traffic engineer or other traffic professional acceptable to the Community Development Director; or
- sufficient implementation of transportation demand management measures.

Mixed-Use Parking Allowance Standards

- Exempt the first 1,000 square feet of gross commercial floor area in a mixed-use development from providing off-street parking.
- Allow residential guest spaces to be fulfilled through required commercial parking (shared).
- Allow parking requirements to be reduced up to 50%, subject to a conditional use permit and a required parking study (completed by the City and paid for by the applicant/developer) affirming the parking reduction.
- Require reduction in minimum parking requirements requests to include a transportation demand management plan or other adequate survey data. All requests will be evaluated on a case-by-case basis by the Community Development Director/Designee.

Institutional Uses' Shared Parking Use Opportunities and Parking Facilities Consolidation

Institutional uses such as public auditoriums, civic clubs, religious institutions, and medical facilities with large, occasionally used, parking lots are encouraged to enter into shared parking agreements with surrounding property owners to increase the parking lot's daily utilization, thus limiting the need for additional parking facilities. Institutional uses entering into such agreements may receive a portion of the associated in-lieu fee for the parking facilities operation and maintenance.

Institutional uses with large surface lots are encouraged to consolidate their parking supply into a single parking structure, thereby "freeing" up the surface lots for additional development or public open space. Construction and operation of the parking structure may be funded, in part, by the in-lieu fee fund.

Multifamily Residential, Commercial, Industrial, Institutional, and Mixed-Use Parking Facilities Standards

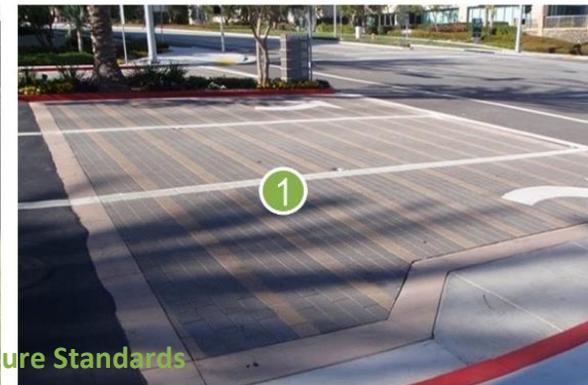
- Locate parking behind buildings and away from the street.
- Avoid or eliminate duplicative driveway entrances.
- Design parking lots by dividing a large parking lot into a series of smaller connected lots to reduce "heat island" effect.
- Incorporate a minimum stacking distance of 40' between the edge of the travel lane and access into the first parking bay.
- Avoid dead-end drive aisles and intersections. When necessary, provide a 5'-wide recessed area in the landscape area for parking at the end of dead-end drive aisles.
- Design onsite circulation system to minimize pedestrian and vehicle conflicts.

Multifamily Residential, Commercial, Industrial, Institutional, and Mixed-Use Parking Facilities Guidelines

- Encourage:
 - first-floor parking within buildings (“tuck under parking”) and underground parking below buildings;
 - reciprocal access drives linking adjacent properties and avoiding individual curb cuts for every use/parking lot;
 - parking access to be located as far as possible from street intersections to allow for adequate queuing and visibility;
 - colored, textured, and permeable pavement treatments at entry driveways;
 - textured paving at crosswalks within the project as opposed to a painted stripe designation, provided it does not conflict with ADA requirements;
 - paving materials, varied in texture and color, to be used where pedestrian and vehicular areas overlap to minimize the negative impact of large expanses of asphalt. The use of concrete (exposed aggregate, colored, or stamped) or pavers (stone, brick, or granite) is preferred;
 - walkways wide enough for multiple people to pass (approximately 8’ in width) and directly connect public sidewalks with sidewalks on private property and be oriented toward building entrances.
 - sidewalks at building entrances to be a minimum of 10’-wide when adjacent to head-in parking, allowing for car bumper overhang, and 8’-wide adjacent to a landscape planter or drive aisles.

Figure 4-39

- ① Crosswalk highlighted with color and texture changes
- ② Paving materials and color variation
- ③ Wide walkways



Parking Structure Standards

- Locate parking structures below, behind, or between buildings.
- Integrate parking structures into the surrounding buildings' architecture, using architectural details to reduce the massing and landscaping to provide visual relief.
- Design access to parking garages:
 - from side streets or alleys, where possible, rather than from pedestrian-oriented streets; and
 - to be integrated into the overall design of the building facade and minimize disruptions along the street frontage.
- Design parking structures to:
 - appear as the same land use type as the primary use's building and be compatible with the surroundings when visible from public right-of-way; and
 - provide screening for cars and headlights while allowing for natural ventilation.
- Design partially submerged parking podiums that project above grade to either be integrated into the architectural character of the building above, using cladding or building with materials that extend down from the portions of the building above, or be built with contrasting materials of a more substantial and permanent character than the portions of the building above to create a base.
- Use architectural details on parking structure to reduce the massing and enhanced landscaping to provide visual relief.
- Incorporate other technologies, such as charging stations for electric vehicles, into parking.
- Provide attractive signs to clearly direct drivers into and out of parking structures and surface parking entrances.

Parking Structure Guidelines

- Encourage the following preferred parking structure design:
 - Wrapped on Ground Level - An above-ground parking structure where non-parking uses, such as retail spaces, are integrated into the ground level of the building along the street frontage of the parcel. The parking structure may be exposed to the building street frontage on upper levels, with appropriate design and screening.
 - Wrapped on All Levels - An above-ground parking structure where non-parking uses are integrated into the building along the entire street frontage of the parcel on all levels of the building. The parking structure is totally hidden behind a "liner building" of non-parking uses.
 - Underground (Partial or Entirely) - A parking structure that is fully submerged underground and is not visible from the street. Depending on amount of parking provided, it may also include a level of at-grade parking hidden behind non-parking uses such as retail.
- Encourage the installation of solar panels on roof-decks of parking structures and carports in surface lots, both as shading devices for vehicles and as a sustainable energy source.
- Encourage electric vehicle and bicycle charging stations.

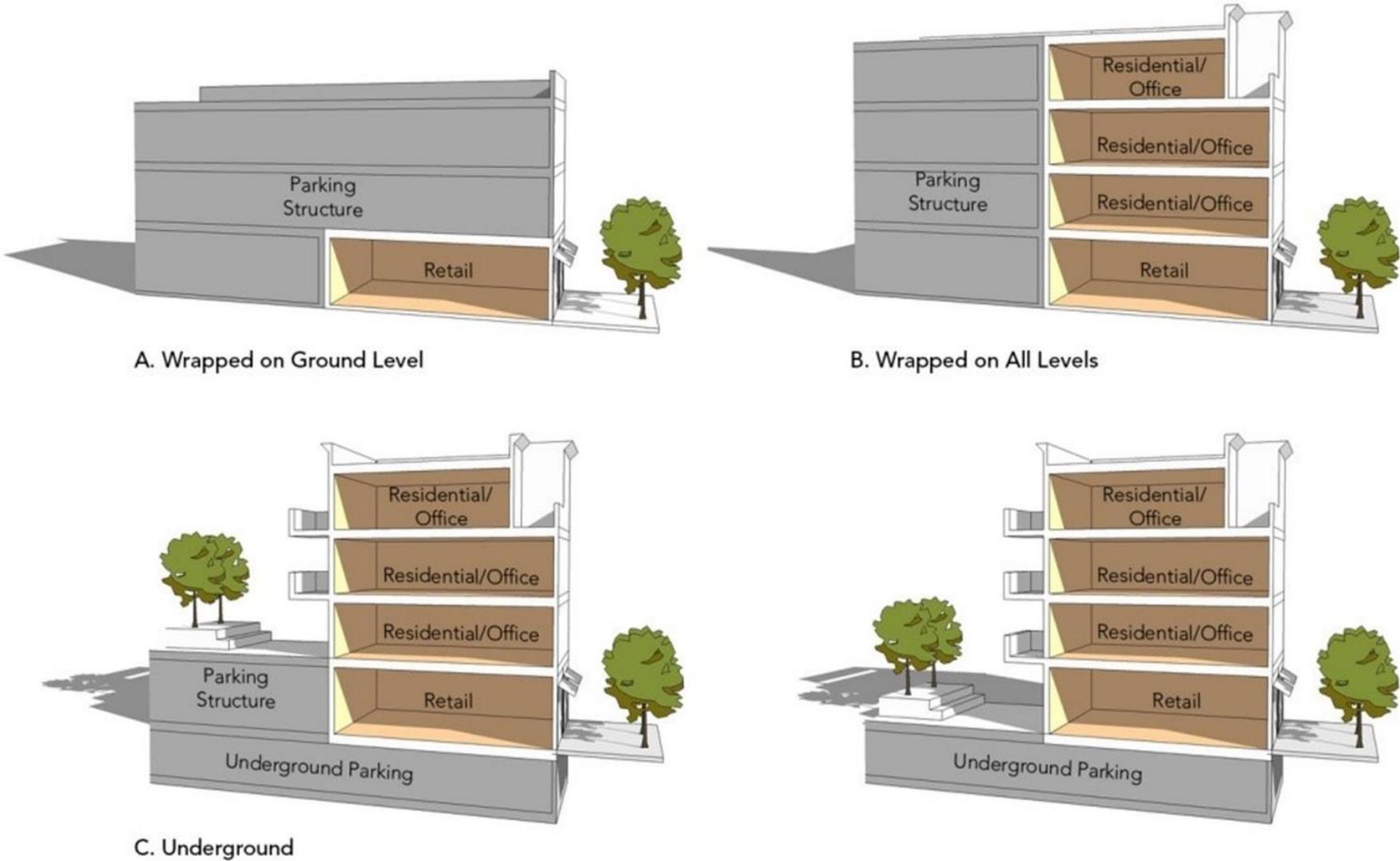
Parking | Parking Structure Guidelines

Figure 4-40

- 1 “Wrapped” structure integrated with retail, architectural detail, landscape “green wall”
- 2 Screening allowing ventilation
- 3 Clear and visible entries
- 4 Architectural detail
- 5 Sense of vehicle and pedestrian entry



Figure 4-41



Uncovered Parking Area Landscaping Standards

- Require:
 - 5'-wide (minimum) landscape finger planter at the end of each parking aisle;
 - canopy trees or solar shade structures to be installed to provide shade. Trees should have a 30- to 40-foot canopy potential and be sized at 24-inch box at the time of installation;
 - 1 tree for every 4 parking spaces (minimum tree well size of 5' by 5'); and
 - 2' (minimum) in the planted area for vehicle overhang. This area may be counted as part of the length of the parking stall, but not as part of required planter area.

- ① Shade trees
- ② Fragrant trees
- ③ Finger planters
- ④ Low hedges/shrubs



Uncovered Parking Area Landscaping Guidelines

- Encourage planting areas with low hedges and shrubs (approximately 3' high) to assist in screening parking lots from public view where parking is adjacent to a street.
- Recommend the use of pervious paving materials, open pave blocks, and intermittent paving to reduce the amount of runoff and retain water for onsite irrigation.

Figure 4-42



Sustainability

To promote a built environment that will have a positive impact on the community with respect to the natural environment, local economy, and the health and productivity of the residents, it is highly recommended that all projects incorporate the sustainable design criteria below.

Site and Building Design Standards

- Design and construct energy efficient buildings to reduce air, water, and land pollution and environmental impacts from energy production and consumption.
- Provide shade structures and trees that produce large canopies to reduce the heat-island effect. In addition, choose roof and paving materials that possess a high level of solar reflectivity (cool roofs).
- Use recycled and other environmentally-friendly building materials.
- Provide EV chargers.

Solar Access and Orientation Standards

- Use passive and active solar strategies in building and site design and construction.
- Orient buildings to take advantage for solar gain, thus allowing the absorption of the sun's heat during colder months, while minimizing the sun's heat during warmer months.

Solar Access and Orientation Guidelines

- Encourage solar panels into the design for all structures.
- Promote thermal mass to return the sun's warmth during cooler months.
- Recommend long overhangs to screen summer sun and allow for solar gain in the winter.
- Orient buildings on an east-west axis to minimize western exposure.

Stormwater Management Standards

- Install filter strips along streets and parking lots to reduce urban runoff.
- Integrate permeable pavement and perforated curbs throughout the project area to allow stormwater to enter planter area and ultimately help with filtration and runoff.
- Use pervious paving materials, open pave blocks, and intermittent paving to reduce the amount of runoff and retain for irrigation.
- Incorporate onsite features, such as porous pavement, bio-retention, and wet ponds for capturing and directing stormwater runoff.
- Use rain water catchment systems to minimize runoff.
- Use landscaped areas including parks, parklets, bulb-outs, and park walkways as swales to assist with bio-filtration and reduction of urban runoff.
- Use native and drought tolerant plants to reduce water demand.

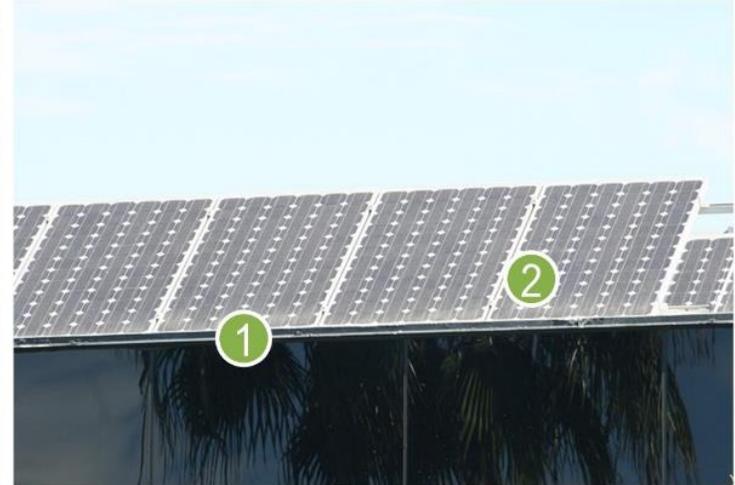
Stormwater Management Guidelines

- Make "saucers" (tree well/basin) around newly planted trees and shrubs.
- Encourage the harvest of stormwater runoff using surface grading.
- Recommend vegetated swales and similar methods into the natural drainage patterns/flow lines to convey runoff towards basins or other collection areas.

Sustainability

- 1 Shade structures
- 2 Solar panels
- 3 Drought-tolerant plants

Figure 4-43



Sustainability

- 1 Vegetated rain catchment system
- 2 Landscaped bulbouts
- 3 Native or drought-tolerant plants

Figure 4-44



COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 5: MOBILITY AND INFRASTRUCTURE





Chapter 5 Contents

- Overarching Objectives and Vision
- Street Network
- Covina Metrolink Station Concept
- Citrus Ave Concept
- North 2nd Ave Concept
- East Front Street Concept
- Health Corridor Concept
- Pedestrian
- Bicycle
- Transit
- Parking Management
- Water Infrastructure
- Sanitary Sewer Infrastructure
- Storm Drainage Infrastructure
- Dry Utilities Infrastructure

CHAPTER 5: MOBILITY AND INFRASTRUCTURE

INTRODUCTION

Covina's streets and infrastructure have evolved over a century of growth and development. Well-designed streets improve residents' quality of life by creating a circulation network that is **accessible, people-oriented, multi-modal, and interconnected**. This chapter outlines the strategy for creating an enhanced circulation network in the Covina Town Center, where the needs of pedestrians, bicyclists, and transit users are put into better balance with motor vehicle users.

A **cost-efficient and reliable infrastructure network** is essential to serve the needs of residents and businesses without compromising the character of the Covina Town Center. Covina's health, safety, livability, and economic prosperity are dependent on its network and availability of wet and dry utilities that serve the community daily. As the Covina Town Center continues to change in density and intensity of development, the City's focus will be on providing an adequate level of services to accommodate existing uses and projected growth while using fewer resources.

The strategies presented in this chapter – including roadway improvements on key streets, the addition of new streetscape elements, and the expansion of bicycle, pedestrian, and transit facilities – will help transform Covina's Town Center into a place that bustles with life and activity.



OVERARCHING OBJECTIVES AND VISION

The following overarching objectives define the Covina Town Center’s **long-term mobility and infrastructure direction to support anticipated local and regional growth**. These broad policy statements will guide future public agency decisions related to mobility, street design, and infrastructure in the Covina Town Center.

- Improve connectivity to the Covina Metrolink Station
 - Improve pedestrian facilities to create comfortable walking environments that promote pedestrian activity.
 - Optimize the street right-of-way to accommodate multi-modal services and amenities.
 - Incorporate curb extensions (“bulb-outs”) to increase pedestrian visibility and safety at crosswalks, calm traffic speeds, and provide space for rain gardens, tree plantings, street furnishings, and other amenities.
 - Enhance transit stops with shade structures, seating, bus information, and other amenities.
 - Promote efficient, effective, and equitable provision of high-quality utilities and infrastructure throughout the Covina Town Center.
- Maximize existing infrastructure resources and plan for future demands.
 - Work with utility providers to coordinate future infrastructure improvements to address potential utility deficiencies.
 - Coordinate future Covina Town Center improvement projects to maximize the use of public and private funding for infrastructure improvements.

STREET NETWORK

Citrus Avenue, San Bernardino Road, and Badillo Street form the Covina Town Center’s primary street network. These streets **each serve related but distinctly different purposes**. Citrus Avenue provides north/south regional connectivity to the surrounding communities of Glendora and West Covina and access to the I-210 and the I-10 freeways. San Bernardino Road and Badillo Street provide east/west connectivity to neighborhoods within Covina and adjacent cities. Badillo Street also accesses SR-57 freeway (via Covina Boulevard).

Street Types

Street types are defined by the primary mode or modes of transportation, traffic speed, and overall character of the street. Street types in the Covina Town Center, as identified in the General Plan, include **arterial roadways, collector streets, and local streets** (see sidebar). The street network connects Covina regionally but can pose a significant physical barrier to pedestrians and bicyclists locally, if not planned to sensitively accommodate a range of travel modes.

Complete Streets

The term “**complete streets**” describes a comprehensive approach to the practice of mobility planning. Complete streets principles recognize that transportation corridors have multiple users with different abilities and mode preferences (driving, biking, walking, and taking transit). Adjacent land uses influence the functionality and character of the street environment. A well-integrated street system considers the **complementary relationship between land use, local and regional travel needs, and the greater community context**. Complete streets consider the broadest range of users, including children, seniors, and those with special needs. Such specially designed streets can accommodate expected traffic demand while also providing additional facilities to support travel by other modes. In addition, complete streets can contribute to the creation of vibrant public spaces by incorporating distinctive placemaking and programming elements.

The principles of complete streets are an integral part of the Covina Town Center Specific Plan (CTCSP) and work together with land use policies that encourage economic development. The following recommendations, categorized by key streets and street sections, provide for a

transportation network that successfully integrates bicyclists, pedestrians, and transit users, along with vehicle drivers.

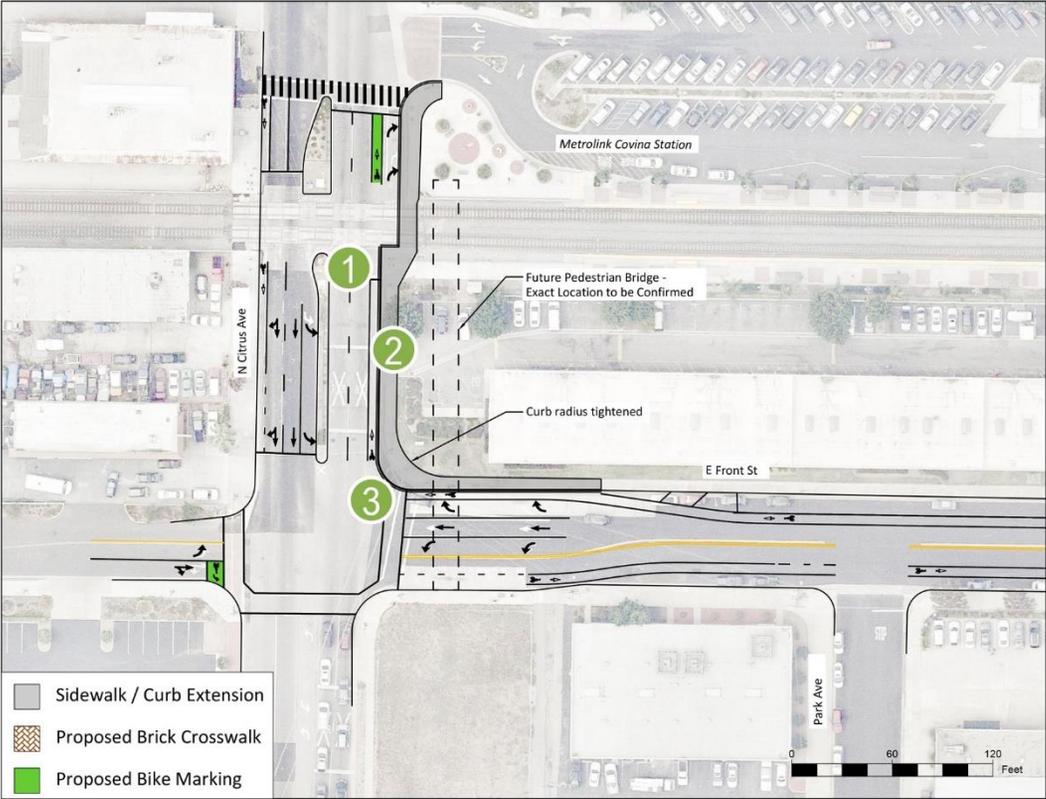
COVINA METROLINK STATION CONCEPT

Improving connectivity between the Covina Metrolink Station and the Covina Town Center is imperative to enhance safety, increase Metrolink ridership, reduce traffic congestion, and improve the Covina Town Center’s viability and vibrancy. A series of roadway improvements to North Citrus Avenue and East Front Street are necessary to better accommodate the high pedestrian volume between the Covina Metrolink Station’s parking structure and the Covina Metrolink Station, to improve connectivity to the Covina Town Center, and encourage more walking and bicycling activity.

North Citrus Avenue, East Front Street, and Covina Metrolink Station Enhancements

Figure 5-1

- 1 Narrowed north-bound lanes, north of Front Street
- 2 Increased sidewalk widths 4’ (if opportunity arises)
- 3 No right-on-red



Widened Sidewalk/Curb Extensions (revised 9-20-2022)

Current sidewalk width in this area is minimal when considering the high pedestrian volume between the parking garage and Metrolink station. Conditions are further constrained by the cluster of sign poles and utilities near the railroad crossing.

- Improve pedestrian access through the addition of up to 4' of sidewalk width on the east side of North Citrus Avenue to improve pedestrian access.
- Narrow the northbound automobile lane widths on North Citrus Avenue, north of East Front Street, to enable the widened sidewalk/curb extension.

Crosswalks

Install highly visible crosswalks at the North Citrus Avenue/East Front Street and North Citrus Avenue/Covina Station intersections. Whether continuing the interlocking brick textured crosswalk patterns that exist in the Covina Town Center core, or following a more conventional application of bright, reflective "continental" or "ladder" crossings, improving the visibility of these crosswalks will provide more warning to oncoming drivers and will draw pedestrians to marked crossings. These enhancements are intended to reduce jaywalking at unsafe locations and discourage vehicles from stopping in clearly marked pedestrian zones.

Pedestrian Bridge

Construct a pedestrian bridge crossing East Front Street and rail tracks to the Metrolink Station. Refer to Pages 2-7 and 2-8, and figure 2-2 of Chapter 2 Visions, Frameworks, Strategy regarding the pedestrian bridge.

Signalization

- Institute a "leading pedestrian interval" at the North Citrus Avenue/East Front Street intersection signal. Adding a leading pedestrian interval (LPI) phase to the current signal operations at this intersection addresses conflicts between the high volume of pedestrian and automobile traffic. The LPI phase should provide a minimum of five seconds of advance crossing time for pedestrians, enabling them to establish a visible position in the crosswalk by the

time vehicles receive the "green" signal. This improvement will also reduce the amount of "green" time that turning vehicles will spend queuing inside the intersection by clearing pedestrians toward the front end of the cycle. The LPI phase can be triggered automatically each pedestrian phase or by push button activation.

- Remove push button pedestrian signal activation requirement. While push button activation may help expedite automobile traffic when pedestrians are not present, it can also be a significant pedestrian impediment when not used or functioning properly. Removing the requirement means that the pedestrian "walk" signal will trigger, even if not pressed. This will account for instances when pedestrians are present, but do not press the button, or when the button is not functioning properly.
- Consider prohibiting right-turn-on-red movements and instituting a pedestrian "scramble" or pedestrian-only signal phase.

LPI (Leading Pedestrian Interval) is a signal system where the pedestrians are given a "head start" entering the crosswalk or intersection ahead of the green light for vehicular traffic. LPIs have been shown to reduce pedestrian-vehicular collisions because the pedestrians are more easily seen by the vehicle drivers.

Bicycle Improvement

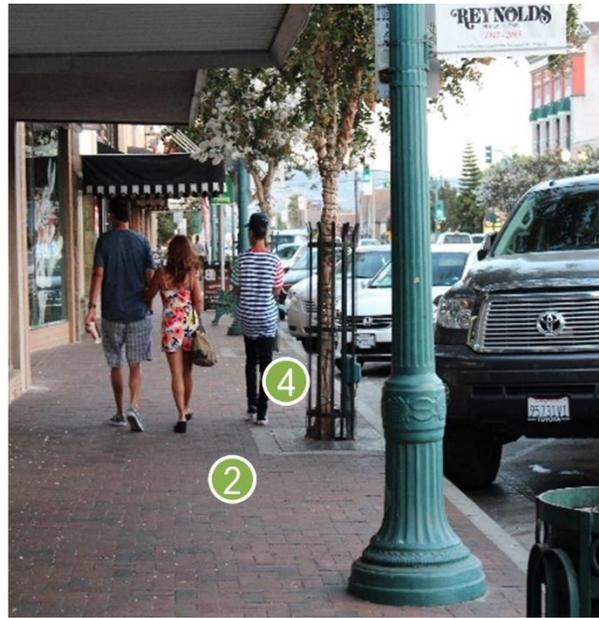
Paint a green bicycle lane between the railroad tracks and the Covina Metrolink Station entrance.

Streetscape Improvements

- Provide streetscape improvements on North Citrus Avenue between San Bernardino Road and East Front Street. The large expanses of surface parking and vacant land create an inhospitable walking environment that reinforces a disconnect between the Covina Station and the Covina Town Center core. Sidewalk widths are large enough to install a 4' - 6' landscape

or amenity strip, while allowing for 6' of clear space. Provide ongoing maintenance of existing streetscape improvements or amenities such as bicycle racks.

- Extend the landscaping and streetscape amenities that mark the North Citrus Avenue area south of San Bernardino Road into the two-block area to create a contiguous pedestrian environment and facilitate walking. Amenities may include walking trail markers or other wayfinding signs. The landscaping and streetscape improvements will also help to calm traffic by visually narrowing the roadway, despite no actual reduction in capacity.



- 1 Placemaking, pedestrian-scale lighting
- 2 Enhanced sidewalk
- 3 Bicycle racks
- 4 Pedestrian-oriented streetscape

CITRUS AVENUE CONCEPT

Citrus Avenue forms the backbone of central Covina. Citrus Avenue has both the second highest automobile volumes and the highest pedestrian volumes. Yet, street crossings at primary intersections are dominated by automobile movements, with excess roadway (curb-to-curb) widths and wide turning radii. Mid-block crossings are currently provided at several locations, aligning a network of alleys that enhance pedestrian connectivity; however, the crossings can be enhanced to increase visibility (through the use of pedestrian crossing enhancements such as crossing lighting) and create additional amenity space for pedestrians and other users. The enhanced streetscape, including parklets and bulb-outs, **encourage pedestrian activity and allow for increased activities such as outdoor dining.**

As new development brings more pedestrian activity along Citrus Avenue beyond the historic core, it is important to provide an **enhanced pedestrian environment** north of San Bernardino Road and south of Badillo Street. The following improvements are inherent components of the CTCSP and will be required to facilitate the vision:

- Enhance the sense of place and improve safety through the installation of pedestrian-scale lighting along the sidewalk. For the installation of new lighting, the City will review existing lighting conditions and focus new lighting especially at pedestrian concentration areas, such as bus stops and crosswalks.
- Encourage walking throughout the district with maintenance and installation of additional shade trees and ornamental trees (where gaps in the canopy exist), bulb-outs, and stormwater planters. In addition to enhancing the walking experience, street trees help define the area and will continue to contribute to the overall character of the Covina Town Center.
- Facilitate the installation of “parklets” within the public right-of-way. These small green spaces can often fit within the public right-of-way, replacing underutilized parking spaces or parts of the sidewalk with planted and other passive spaces for people to sit, linger, and watch passersby.
- Consider relocating the West Badillo/Covina Park bus stop farther east to prevent buses from blocking 3rd Avenue.

Curb Extensions/Bulb-outs

Curb extensions (or “**bulb-outs**”) **will be added to key locations** to narrow the roadway, calm traffic, and in certain cases shorten pedestrian crossing distances. At intersection corners, curb extensions reduce and tighten turning curb radii, encouraging slower turning speeds. Underutilized portions of the parking lane adjacent to bulb-outs may be designed for stormwater planters to provide **opportunities for green infrastructure** or to allow for **parklets or even outdoor dining space**. Specifically, install curb extensions/bulb-outs on all four corners at Citrus Avenue signalized intersections -- San Bernardino Road, Cottage Street/Italia Street, College Street, and Badillo Street. Each of these intersections have excess roadway width that can be converted to pedestrian space. Extending curbs at these locations will also:

- Increase visibility of pedestrians prior to crossing, and
- Define curbside parking zones and prevent parking in “red” zones.

Key issues to consider in the design of all curb extensions include drainage, bus access, parking, and fire hydrants. Curb extensions at intersection locations have no minimum length but must be designed to cover at a minimum the width of the crosswalk (where present) and be no less than 100 square feet in size.

Curb Extension/Bulb-out Images Figure 5-3



- 1 Restaurant or pedestrian-oriented use adjacent to or on bulb-out
- 2 Landscaped bulb-out
- 3 ADA compliant

North Citrus Avenue/San Bernardino Road Pedestrian Enhancements

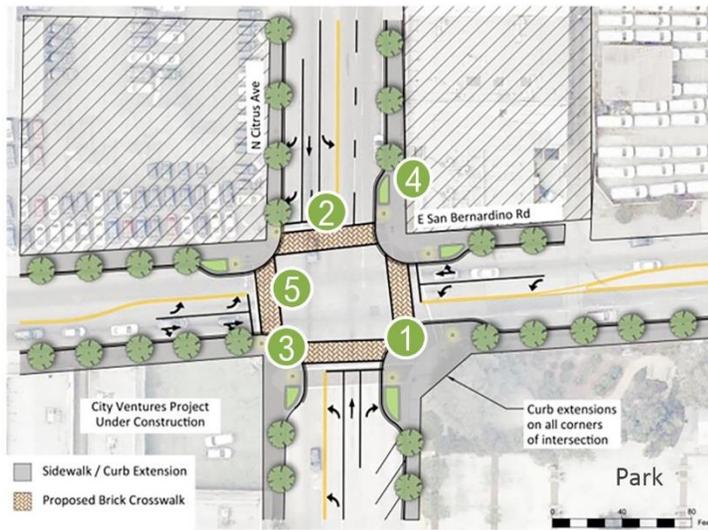


Figure 5-4

- 1 Bulb-outs
- 2 Flashing in-street pedestrian crossing lights
- 3 Pedestrian-scaled lighting
- 4 Shade trees/stormwater planters
- 5 Highly visible crosswalks

North Citrus Avenue/Badillo Street Pedestrian and Bicycle Enhancements

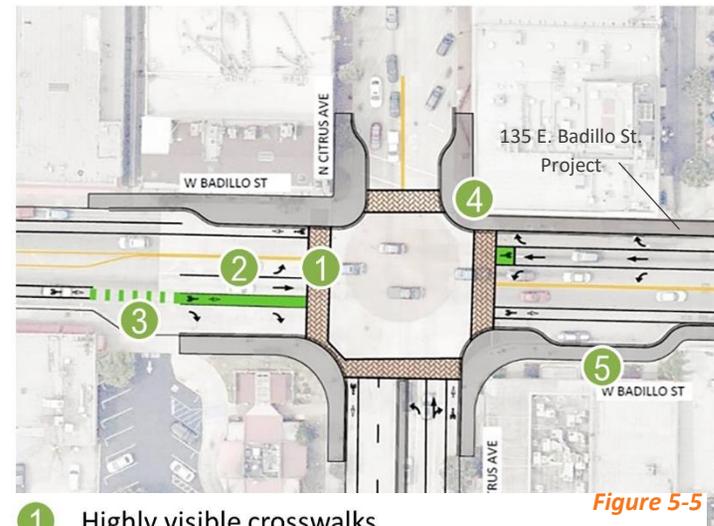


Figure 5-5

- 1 Highly visible crosswalks
- 2 Lane modification
- 3 Bike sharrows
- 4 Bulb-outs
- 5 Bus turnout

Crosswalks

Install **highly-visible crosswalks** at the North Citrus Avenue/San Bernardino Road and North Citrus Avenue/Badillo Street intersections, whether continuing the interlocking brick textured North Citrus Avenue crosswalk patterns or a more conventional application of bright, reflective “continental” or “ladder” crossings.

Mid-Block Crossings

Mid-block **curb extensions and crossings help slow speeds on Citrus Avenue and provide more area for pedestrians, street trees, and other amenities as well as help meet regulatory water quality objectives through the use of permeable surfaces.** Extend curbs and provide additional signage at the Citrus Avenue mid-block crossings. Install Rectangular Rapid-Flashing Beacons (RRFBs) or other measures to improve the visibility of the pedestrian crossings.

Signalization

- Add a leading pedestrian interval (LPI) phase to the current signal operations at the Citrus Avenue intersections with San Bernardino Road, Cottage Street/Italia Street, College Street, and Badillo Street to address conflicts between the high volume of pedestrian and automobile traffic at these intersections.
- Remove the push button pedestrian signal activation requirement.

Mid-Block Crossing Images



Figure 5-6

- 1 Pedestrian-oriented lighting and bulb-out
- 2 Planted bulb-out facilitating stormwater drainage and increased filtration
- 3 Highly visible crosswalk with pedestrian in-street lighting

Citrus Avenue Food Hall Concepts



Figure 5-7



Figure 5-8



Figure 5-9



Figure 5-10

NORTH 2ND AVENUE CONCEPT

North 2nd Avenue can be characterized as a very wide roadway that discourages pedestrian activity even though the Covina Library, local churches, and other public-serving uses front the street. North 2nd Avenue provides a significant opportunity for placemaking and creating a vibrant cultural corridor in the heart of the Covina Town Center. The North 2nd Avenue concept looks to **strengthen the public space with the installation of multimodal amenities and enhanced landscaping, while repurposing the oversized automobile facilities.** These upgrades will accommodate enhanced pedestrian connectivity by slowing traffic, shortening crossing widths, and creating space for additional trees and amenities.

Roadway Modifications for North 2nd Avenue

- Reduce the five vehicular lanes to:
 - Three travel lanes (11'- 16') from Badillo Street to East San Bernardino Road, and
 - Two travel lanes from East San Bernardino Road to East Front Street.
- Provide two 8' on-street parking lanes.
- Install 10' sidewalks, expanding to 16' for pedestrian plazas and parks.
- Enhance parkways with trees, ground cover, and shrubs.

- Construct a landscaped median from Badillo Street to East School Street and from East San Bernardino Road to East Front Street.
- Install protected bike lanes ("cycletracks") between East San Bernardino Road to East Front Street.
- Maintain key access points to the Fire and Police Departments' facilities. Do not construct a landscape median in front of the Fire and Police Departments' facilities.
- Install Library Pedestrian Plaza.

Transit Amenities

Improve transit amenities along 2nd Avenue including **bus shelters and seating, waiting areas, and boarding zones.**

Covina Library Pedestrian Plaza

Establish a pedestrian plaza at the North 2nd Avenue/East Italia Street intersection to create a **local focal point for the Civic/Cultural district.** The plaza area should be expanded to a minimum of 16' and be landscaped and furnished with outdoor tables, benches, and other amenities that encourages outdoor reading and community gathering space. Look to establish plaza or other pedestrian amenities on all corners of North 2nd Avenue/East Italia Street.

North 2nd Avenue Sample Cross Section

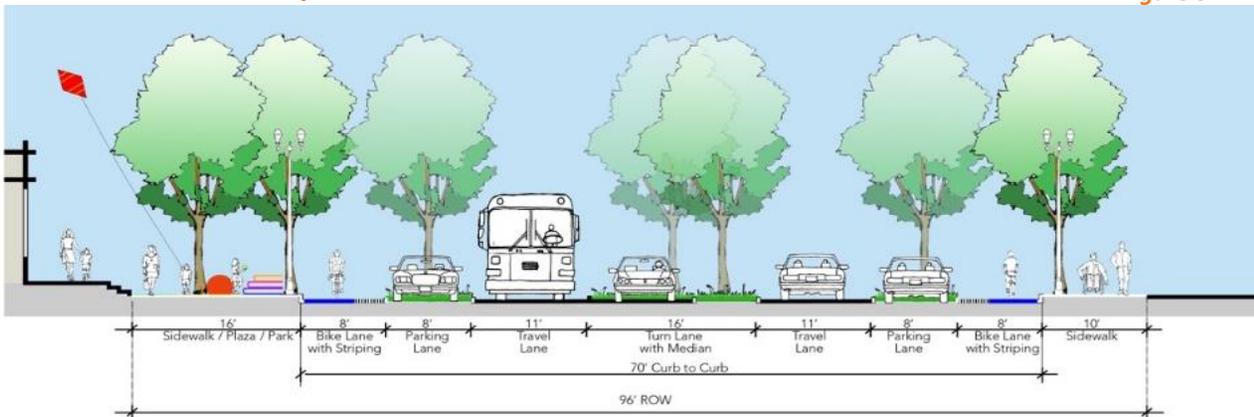


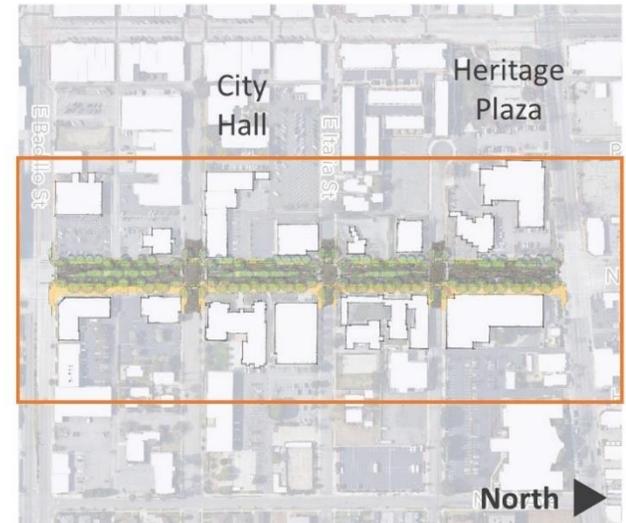
Figure 5-11

icements

Figure 5-12



- ① Lane modifications
- ② Sidewalk installation
- ③ Plazas/parks
- ④ Landscaped parkways
- ⑤ Landscaped medians
- ⑥ Cycletrack



EAST FRONT STREET CONCEPT

East Front Street currently serves a variety of automotive and light industrial uses that generate little pedestrian activity and often conflicts with pedestrian movement through and within the corridor. East Front Street's size and character varies greatly between North Citrus Avenue and Barranca Avenue. Specifically, roadway widths vary from 60' to 40' with some sections providing sidewalks, curbs/gutters, and on-street parking. **As new development in Front Street/TOD/F.A.I.R. district evolves, East Front Street's importance as a multimodal corridor will emerge.**

Sidewalk Network

Complete the noncontiguous sidewalk in areas where gaps exist, which include:

- Small sections on both sides of East Front Street, east of North Howard Avenue. A minimum 5' unobscured sidewalk should be installed and maintained with new curbs to demarcate the separation between pedestrian and vehicular zones; and
- Sections on the East Front Street's south blockface, between North Curtis Avenue and Barranca Avenue. A minimum 5' sidewalk should be established in this area, recognizing that utility infrastructure may require adaptations.

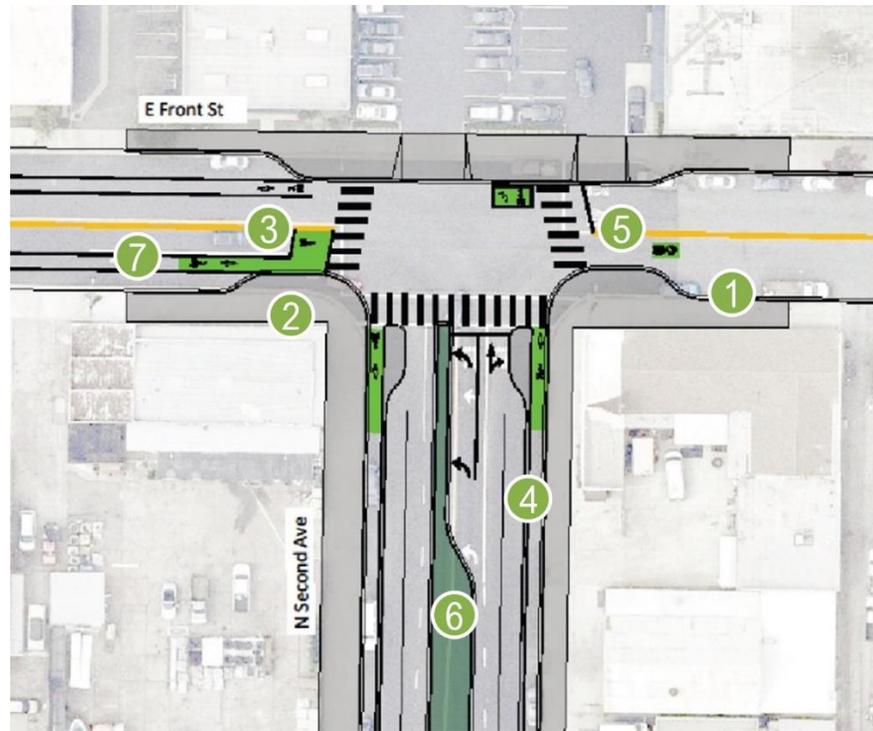
Defined Street Profile and Edges

East Front Street, immediately east of North Citrus Avenue, is at its widest at this location (60'). This section is edged with sidewalks, concrete curbs, curb cut driveways, and on-street parking. East of 2nd Avenue, however, East Front Street's profile changes multiple times, narrowing at the eastern end to 40'. The most problematic section, between North Howard Avenue and North Curtis Avenue, has undefined curbs with private land uses spilling into the right-of-way.

Establish a uniform 75' right-of-way and 50' roadway width to provide space for **pedestrian amenities or other multimodal improvements** to support a vibrant mixture of uses.

Intersection Enhancements: East Front Street and North 2nd Avenue

With the development of the Front Street/TOD/F.A.I.R. District, East Front Street's character will change. The **East Front Street/North 2nd Avenue intersection will be an important junction** between the higher-volume multimodal corridors to the south and west that serve automobiles, bicycles, and buses, and a more pedestrian-focused district to the east. Enhance the East Front Street/North 2nd Avenue intersection to include curb extensions/bulb-outs on the intersection's east blockface, thus, complementing the North 2nd Avenue enhancements, facilitating safe crossings, and creating placemaking opportunities in the Front Street/TOD/F.A.I.R. district.



- 1 Consistent sidewalks along E Front St
- 2 Bulb-outs
- 3 Bike boxes
- 4 Cycle track
- 5 Uniform road with right-of-way
- 6 Landscaped median
- 7 Bike lane

Bicycle Improvements

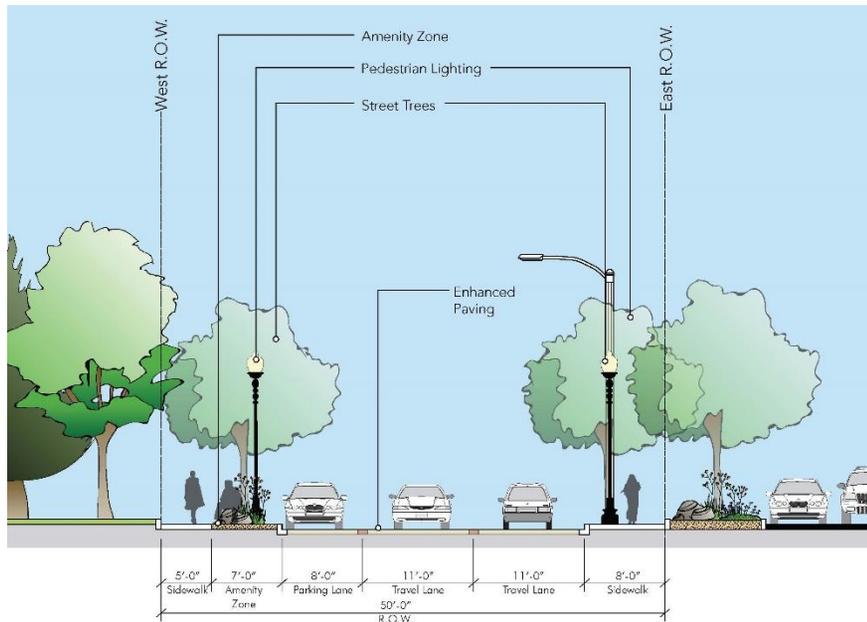
- Install bike boxes for eastbound and southbound cyclists and install a protected two-stage turn box for cyclists heading west, smoothing the transition between North 2nd Avenue's proposed cycletrack and East Front Street's bike lanes.

HEALTH CORRIDOR CONCEPT: NORTH 3RD AVENUE, WEST COTTAGE DRIVE, AND WEST COLLEGE STREET

Covina Town Center’s Health Corridor extends from Emanate Health Inter-Community Hospital and Covina Park into the heart of the Citrus Avenue historic district. While the hospital campus is densely built, the surrounding blocks are dominated by a preponderance of surface parking lots, undermining pedestrian connectivity in the short distance between North Citrus Avenue, the medical campus, and Covina Park. The following recommendations for North 3rd Avenue, West Cottage Drive, and West College streets **will improve the pedestrian connectivity and set the stage for pedestrian-oriented infill development in the future.**

Establishing the North 3rd Avenue public plaza will create a new front door to the hospital and medical uses. A series of small to large incremental investments along North 3rd Avenue will transform the street function and create a new, multi-functional public space between West Cottage Drive and West College Street.

Figure 5-14



North 3rd Avenue

Curb Extensions/Bulb-outs

Install curb extensions/bulb-outs at the intersections of West Orange Street, West Cottage Drive, and West College Street with North 3rd Avenue to slow north-south traffic, shorten crossing distances, and frame on-street parking zones.

Mid-Block Crosswalk

Add a raised, mid-block crosswalk on North 3rd Avenue between West Cottage Drive and West College Street, which aligns with the pedestrian spine extending from Covina Park, through the hospital campus, and across the hospital parking lots to Citrus Avenue. Mid-block crossing visibility can be improved through the use of enhancements such as in-street crossing lighting.

Roadway Narrowing

Narrow North 3rd Avenue’s roadway width between West Cottage Drive and West College Street to 20’, creating a “shared street” with wide pedestrian zones and access for emergency vehicles.

Placemaking Pedestrian Plaza

Consider closing North 3rd Avenue to traffic between West Cottage Drive and West College Street creating a pedestrian plaza and new front door for the hospital. While creating the North 3rd Avenue pedestrian plaza may present challenges – including access to the hospital employee lots and emergency vehicle access to the hospital’s primary drop-off location – interim interventions should be implemented temporarily or as a “pilot” for the longer-term improvements. Interim interventions may begin with paint and removable features such as bollards and planters, which can be replaced with permanent curb extensions and driveway relocations.

West College Street Curb Extensions/Bulb-outs

College Street provides a contiguous pedestrian corridor from Covina Park to the residential neighborhoods east of North 1st Avenue with ample sidewalk space, a fair amount of street tree coverage, and largely supportive land uses. While it will take time for future development to fill the “missing teeth” in the land use fabric along this corridor, the addition of curb extensions at each intersection between Covina Park and North 1st Avenue will help **calm traffic and provide safer crossings for pedestrians**. Additional sidewalk amenities should be installed along the way, including more **shade trees and seating, bike racks, and pedestrian-scale signage and lighting**.

BADILLO STREET

Badillo Street provides significant east-west connectivity both within and beyond the Covina Town Center, and it has substantial roadway width to **accommodate a bicycle facility**. While the 2011 Bicycle Master Plan recommends a Class II bike lane through the Covina Town Center, this Specific Plan recommends installing buffered bike lanes to provide additional separation between cyclists and automobiles. If lane widths or roadway widths allow, adding additional buffer width or buffer features such as reflective breakaway bollards will increase safety and comfort for cyclists.

Health Corridor Enhancements

Figure 5-15



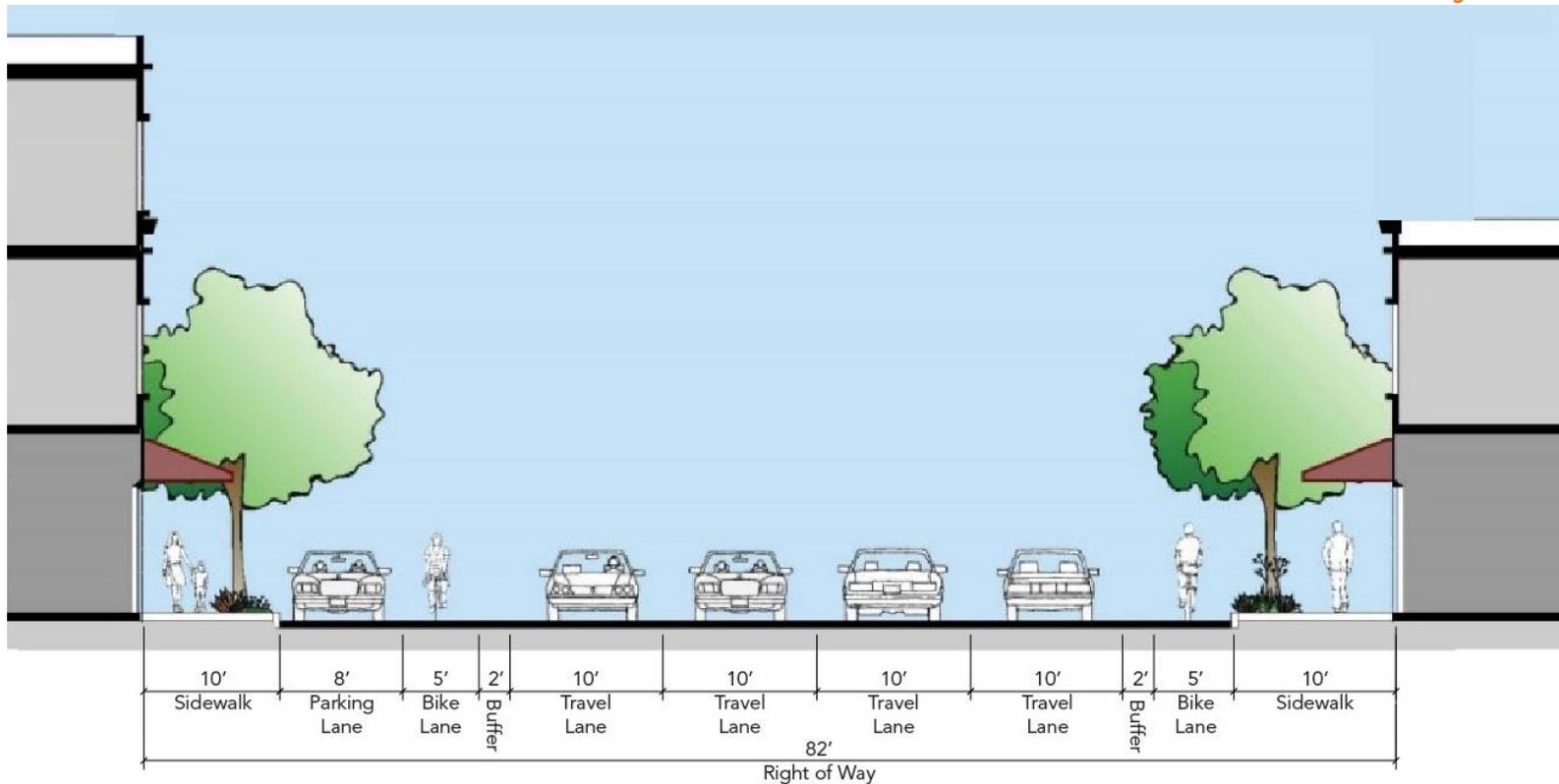
- 1 3rd Avenue Pedestrian Plaza
- 2 Midblock crossing
- 3 Bulb-outs
- 4 Narrowed roadway
- 5 Highly visible crosswalks

EAST SAN BERNARDINO ROAD

East San Bernardino Road also offers east-west connectivity through the north end of the Covina Town Center area. While the roadway has considerably less width than Badillo Street, the Bicycle Master Plan recommends a Class III bicycle route (west of East 2nd Avenue) and a Class II bike lane (east of East 2nd Avenue). The CTCSP recommends installing **buffered bike lanes to provide separation between automobiles and cyclists** due to high automobile volumes and speeds. The buffered bike lanes are accommodated by removing the parking lane from East San Bernardino Road's south side, which is largely underutilized.

East San Bernardino Road Buffered Bike Lane – Sample Cross-Section

Figure 5-16



PEDESTRIAN

A walkable, pedestrian-oriented environment will benefit Covina Town Center residents, business owners, and visitors. The intent of the following policies and actions is to **create a safe, comfortable, and connected pedestrian realm that supports the Covina Town Center's desired sense of place and mobility by people of all physical needs and abilities.**

Pedestrian Facilities

- Install pedestrian facilities including wide sidewalks, curb extensions, parklets, and streetscape improvements (e.g. benches, shade trees, wayfinding, public art, etc.) designed to make walking comfortable, convenient, and interesting.
- Ensure that features, such as lighting, benches, and signage have a cohesive design to establish a unified sense of place.

Network Gaps

- Install sidewalks where gaps in the existing network exist, particularly on East Front Avenue.
- Consider alleys to facilitate improved connectivity.
- Consolidate parking driveways and access points in locations with the fewest pedestrians.
- Improve enforcement of sidewalk obstruction ordinances in areas where abuse is prevalent.

Pedestrian Crosswalks/Curb Extensions

- Shorten crossing distances, increase pedestrian visibility, and decrease vehicle turning speed by modifying curb radii and adding curb extensions.
- Modify pedestrian signals to extend crossing time, provide leading pedestrian intervals, display count-down timers, and function without push-button operation.
- Provide additional signage and pavement markings to alert drivers to upcoming crossings, such as flashing beacons, bold crosswalk patterns, or textured/raised crosswalks at strategic locations.

Universal Design Principles

- Limit changes or obstructions in walking surfaces; create level crossings or ADA-compliant curb ramps at all crosswalk locations.
- Provide pedestrian refuges of at least 6' at crossings of more than four lanes to allow for 2-phase crossings.
- Install auditory pedestrian signals and ensure that crossing times are sufficient for pedestrians of all abilities.
- Include tactile messaging on pedestrian wayfinding and signals to assist travelers with vision impairments.

BICYCLE

The CTCSP provides a **robust bicycle plan connecting the Covina Town Center core with the Covina Metrolink Station**. Bicycle facilities discussed earlier as specific roadway improvements include:

- Buffered bike lanes on East San Bernardino Road, Badillo Street, and North 2nd Avenue between East San Bernardino Road and East Front Street; and,
- Green painted bike lanes and bike boxes on East Front Street between North 2nd Avenue and North Citrus Avenue

In addition to these improvements, the CTCSP calls for improvements addressing “first/last mile” gaps and end of trip facilities.

First/Last Mile Gaps

- Focus on network improvements between proximate origins and destinations including City Hall, Covina Library, public parks and other venues, Emanate Health Inter-Community Hospital, Covina Metrolink Station, and transit hubs.
- Establish a bike share program to facilitate short, convenient trips within the Covina Town Center with emphasis on connecting the Covina Metrolink Station to the heart of the Covina Town Center.
- Provide network signage that communicates proximity and preferred routes.
- Promote the use of bicycle lanes and storage facilities at community events in the Covina Town Center.

End of Trip Facilities Design

- Include bicycle parking in streetscape improvements, whether off-street in an amenity strip, or on-street in the form of a bike corral or similar facility.
- Provide additional public bicycle parking, tune-up stations, and other amenities at key destinations, including Covina Metrolink Station, City Hall, Covina Park, Heritage Park, and the Covina Library.

- In addition to the required on-site bicycle parking, encourage developers to provide additional amenities such as lockers and showers, for longer-distance trips.

Bicycle Use Promotion

- Work with partners to host regular cycling events promoting cycling as fun, convenient, and cost-effective.
- Provide incentives for visitors, commuters, and others to travel by bike with discounts at local businesses, cash-out benefits for employees, and contests that reward bicycle commuters.
- Advertise the availability of bicycle storage availability for community events.

Types of Bikeways

“Class I Bike Path” is a completely separated right-of-way for bicycles and pedestrians with minimal cross traffic from motorists.

“Class II Bike Lane” is a striped lane for one-way bike travel on a street.

“Class III Bike Route” is a shared use route, marked with signs or on-street symbols, but no dedicated lane.

“Class IV Cycletrack” is a separated bikeway for the exclusive use of bicycles.

TRANSIT

Foothill Transit

Foothill Transit operates two local bus routes with relatively low ridership in the Covina Town Center; routes 281 and 190. Route 281 operates along North Second Avenue and North Citrus Avenue with service extending from the Puente Hills Mall in Roland Heights to the City of Glendora. Route 190 operates along Badillo Street and North Citrus Avenue with service extending from the cities of El Monte and Baldwin Park, and Pomona. Both Route 281 and 190 generally operate on a 30-minute headway. Route 190 operates with 15-minute headways during the weekday a.m. and p.m. peak periods. Foothill Transit is proposing to develop a transit center and Park & Ride on the northeast corner of North Citrus Avenue and Covina Boulevard. The transit center will provide an opportunity for Covina residents to easily access both the nearby downtown Covina Metrolink Station and the Azusa Metro Gold Line Station. Sidewalks, bike lanes, and pedestrian enhancements (as described in this Chapter) will be important to **make transit more accessible**.

Metrolink

The Covina Metrolink Station is located at the north end of the Covina Town Center area and provides **direct regional access to Downtown Los Angeles and San Bernardino**. Weekday peak service has 20 to 35-minute headways, with trips roughly hourly throughout the day. Parking is provided at an on-site surface lot (155 spaces) and an adjacent garage (655 spaces), both of which are City-owned. Foothill Transit Route 281 provides service to the station, with stops one block south at North Citrus Avenue and East Front Street.

Transit Strategies

Transit service and facilities improvements will support **improved ridership and decreased automobile travel and parking demand**. The improved service and facilities may also assist with the Covina Town Center’s revitalization. The Covina Metrolink Station is an asset that could be better leveraged with the following strategies:

- Work with Foothill Transit to:
 - Locate Route 281’s primary stops at or adjacent to the Covina Metrolink Station to better serve transfers between the services;
 - Develop standards and build bus stops that incorporate Universal Design criteria and amenities such as shelter, lighting, seating, trash receptacles, and rider information;
 - Provide improved bus boarding zone; and
 - Consider the installation of bus bulb-outs.
- Allow the shared use of resources like the City’s Metrolink garage to facilitate the development of nearby underutilized or vacant properties.
- Adopt transit-oriented zoning policies outside of the CTCSP area that support density along transit lines and encourage the provision of affordable/moderate rate housing in proximity to transit.
- Develop and distribute transit information guides to residents, employees, employers, and visitors via the City’s website; at the Covina Metrolink Station and Foothill Transit bus stops, the Emanate Health Inter-Community Hospital, and key public facilities (City Hall, Covina Library, Covina Park, etc.).
- Work with Foothill Transit and private partners, such as bike share, car share, and transportation network companies (TNCs) to develop “mobility hub” improvements at the Covina Metrolink Station. Mobility hub improvements include TNC pick-up/drop-off, bike share hubs, car share vehicles, and temporary, “pop-up” food and drink vendors for riders as appropriate.

Improved Bus Boarding Zones

- 1 Ensure the waiting passengers can be seen from the outside by using glass or open design for the back wall. Include lighting in the shelter, or locate shelters in a well-lit area.
- 2 Pole and bus stop signs must indicate critical information including stop name, route number, stop number, direction or destination, and system logo



Source: nacto.org

Improved Bus Stops



Figure 5-18

- | | |
|-------------------|-------------------------|
| 1 Shade structure | 3 Public art on shelter |
| 2 Wayfinding maps | 4 Protected seating |

Covina Metrolink Station Images

Figure 5-19



PARKING MANAGEMENT

The Covina Town Center’s public parking supply consists of 1,520 on-street spaces and 1,405 off-street spaces. Time limits are the primary parking management tool to yield turnover for on-street parking. On-street time limits vary throughout the Covina Town Center and consist of one, two, and four hours, which likely lead to driver confusion and discourage a “Park Once” environment. Municipal off-street parking consists of eight municipal lots and the Civic Center Garage; all offering free parking for up to four hours on weekdays, and unlimited parking on weekends. The Metrolink Lot and Metrolink garage offer paid parking for Metrolink riders. Overnight parking in municipal lots and on-street is prohibited for non-permit holders.

A comprehensive set of management strategies can help Covina **maximize the existing parking supply, manage demand, balance the needs of all users,** and emphasize cost-effective solutions.

- Support a Park Once strategy, environment, and walkable Covina Town Center.
- Manage public and private parking supply within the Covina Town Center as a shared resource.
- Focus parking measures that ensure availability and access rather than simply increasing supply.
- Expand and retitle Parking District No.1:
 - Expand Parking District No. 1’s boundaries to match the CTCSP boundaries, thereby encouraging alternative parking solutions and minimizing parking oversupply.
 - Retitle the district from “Parking District No. 1” to “Covina Town Center Parking District” (or similar) to help manage and support all modes of transportation.
- Allow in-lieu fee parking payment to fund multimodal and TDM improvements that help reduce Covina Town Center parking demand, with the approval of the Community Development Director.
- Allow reductions to parking requirements for developments that invest in TDM measures, include designated pick up/drop off spots to incentivize ridesharing, or actively encourage autonomous or ride-hailing services,

include designated pick up and drop off spots for ride sharing services, with the approval of the Community Development Director.

- Encourage shared parking as provided in **Chapter 4: Design Guidelines and Standards**.
- Consider:
 - Leasing private parking for public use; and
 - Implementing a program in which private businesses with excess parking supply are encouraged to open their facilities for public use and be compensated.
- Develop a shared parking program that:
 - Creates a parking database identifying supply, demand, and usage;
 - Provides technical assistance on how to enter into agreements and successfully manage shared parking. Potential assistance includes:
 - Initiating negotiations by providing an independent perspective on issues and opportunities, identifying shared-benefit opportunities, and helping to address common concern;
 - Negotiating agreements, including identifying strategic agreement components, such as:
 - ◇ Compensation for increased lot maintenance, lot improvements, added security;
 - ◇ Restricting access to the shared parking, via permits, to area employees to reduce risk and increase accountability; and
 - ◇ Defining any added security or enforcement measures necessary to ensure that the primary users of the facility are prioritized.
- Consider unbundling parking for residential developments with four or more dwelling units in the Front Street/TOD/F.A.I.R. District to lease/sell parking spaces separate from the rental or purchase fees.
- Allow flexible use of curb space to better serve mobility, safety, growth, and placemaking goals, where excess parking supply exists. Potential locations include a portion of the east blockface of North Citrus Avenue bordering Heritage Plaza, West College Street off North Citrus Avenue, and South Citrus Avenue south of Badillo Street.
- Consider pricing parking as demand warrants and occupancy exceeds 85% in the Covina Town Center core.

- Streamline posted parking time limits, so they are easier to understand. Look to relax time limits where possible to encourage Park Once and make spaces more usable. Possible time limits include one-hour time limits in the Covina Town Center core, and four-hour time limits for all other blockfaces that are currently time limited.
- Implement a four-hour time limit on the west blockface of North 3rd Avenue between East San Bernardino Road and Badillo Street.
- Develop a comprehensive communications effort to increase awareness of shared parking opportunities and policies.
- Provide uniform wayfinding and signage directing drivers to public parking facilities and confirming rates, time limits, and enforcement hours.

Wayfinding Images

Figure 5-20



1 Pedestrian and vehicle orientation

2 Easy to read, eye-catching signage

- Monitor autonomous vehicles, ride-hailing, and other mobility technology trends to “future-proof” parking requirements.
 - Ensure parking requirements are flexible enough to allow future viable driving and parking technologies. Requirements that may necessitate adjustments include reduced/eliminated parking supply, convertible or de-constructible parking structures, or increased site loading/unloading areas.

- Consider amending the Zoning Code to reduce parking requirements at sites that encourage autonomous or shared trips, with the approval of the Community Development Director.
- Consider introducing curb management policies to accommodate emerging technologies such as identifying loading space for both deliveries and ride hailing.

Existing Parking Structure Images

Figure 5-21



Metrolink Station



City Hall

WATER INFRASTRUCTURE

To ensure the long-term prosperity of the Covina Town Center, a **balance must be achieved between water use and available water supply**. Planning for such a balance must consider climate variability, supply, and demand. Covina, like many California cities, has limited water resources that cannot easily be expanded and must be carefully managed to serve existing development and provide for future planned development.

Domestic Water Source

Covina Irrigating Company (CIC) is the primary water provider for the City of Covina. CIC's water supply is obtained from the Main San Gabriel Groundwater Basin and from the San Gabriel River. The Metropolitan Water District of Southern California serves as the City's back-up water supplier. This water can be provided through Three Valleys Municipal Water District, a wholesale water agency.

In general, water mains in the Covina Town Center range from 3" to 28". The 2007 Water System Master Plan calls for the replacement of smaller mains.

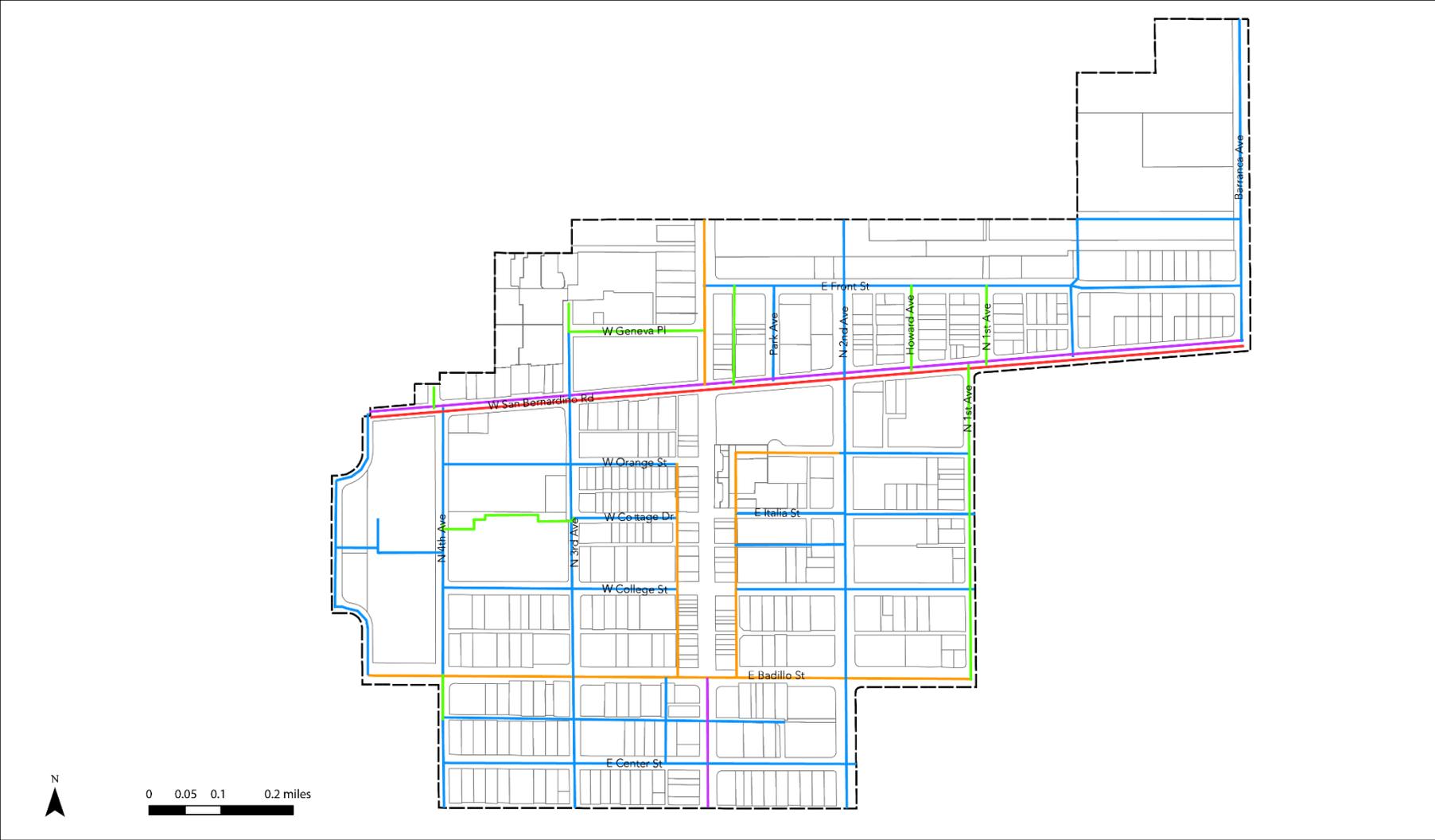
Fire Flow Requirements

Fire flow requirements are based upon building size and building construction type. Fire regulations require all buildings to be equipped with a fire sprinkler system, including certain residential uses. Fire flow requirements may increase due to future regulations and the increase in building size that may be associated with the Specific Plan development allowances. Site plans for proposed projects must be submitted to the Los Angeles County Fire Department to obtain a fire flow requirement and time period based upon the tenant type, building size, and building type.

Water Recommendations

The following improvements are recommended as new development is integrated into the Covina Town Center:

- Maximize efficient use of water resources within the Covina Town Center through conservation, demand reduction, and water recycling.
- Ensure existing water systems are adequate to provide fire protection.
- Encourage projects to incorporate water conservation best management practices including but not limited to low-flow showers and toilets, low-flow and gray-water irrigation systems, and the use of drought-tolerant landscaping.
- Replace aging water lines, as necessary.



LEGEND

<8" Water Pipe (proposed extension to 8")	10" Water Pipe
8" Water Pipe	12" Water Pipe
28" Water Pipe	

Source: 2007 Covina Water System Master Plan

SANITARY SEWER INFRASTRUCTURE

Wastewater – commonly referred to as sewage – is **the water that drains from our sinks, toilets, and showers into the sanitary sewer system**. The sanitary sewer collection system, managed by City of Covina Public Works Department, has gravity sewer lines and a pump station. The local sewers flow into the County Sanitation District (CSD) trunk sewer or into the City of West Covina sewer system, which subsequently discharges into a CSD trunk sewer. Once into the CSD trunk sewer system, the wastewater is treated and disposed.

Existing sewer lines are located throughout the CTCSP area in public street or alley rights-of-way as shown on in the *Sewer Lines Map*. New development projects are reviewed by the City to determine if local sewer lines have sufficient capacity to accommodate an increase in effluent. To account for changes in the existing pipeline network, the City charges new development a fee to upgrade or extend local sewer lines. New development is also required to pay fees to mitigate local wastewater conveyance impacts.

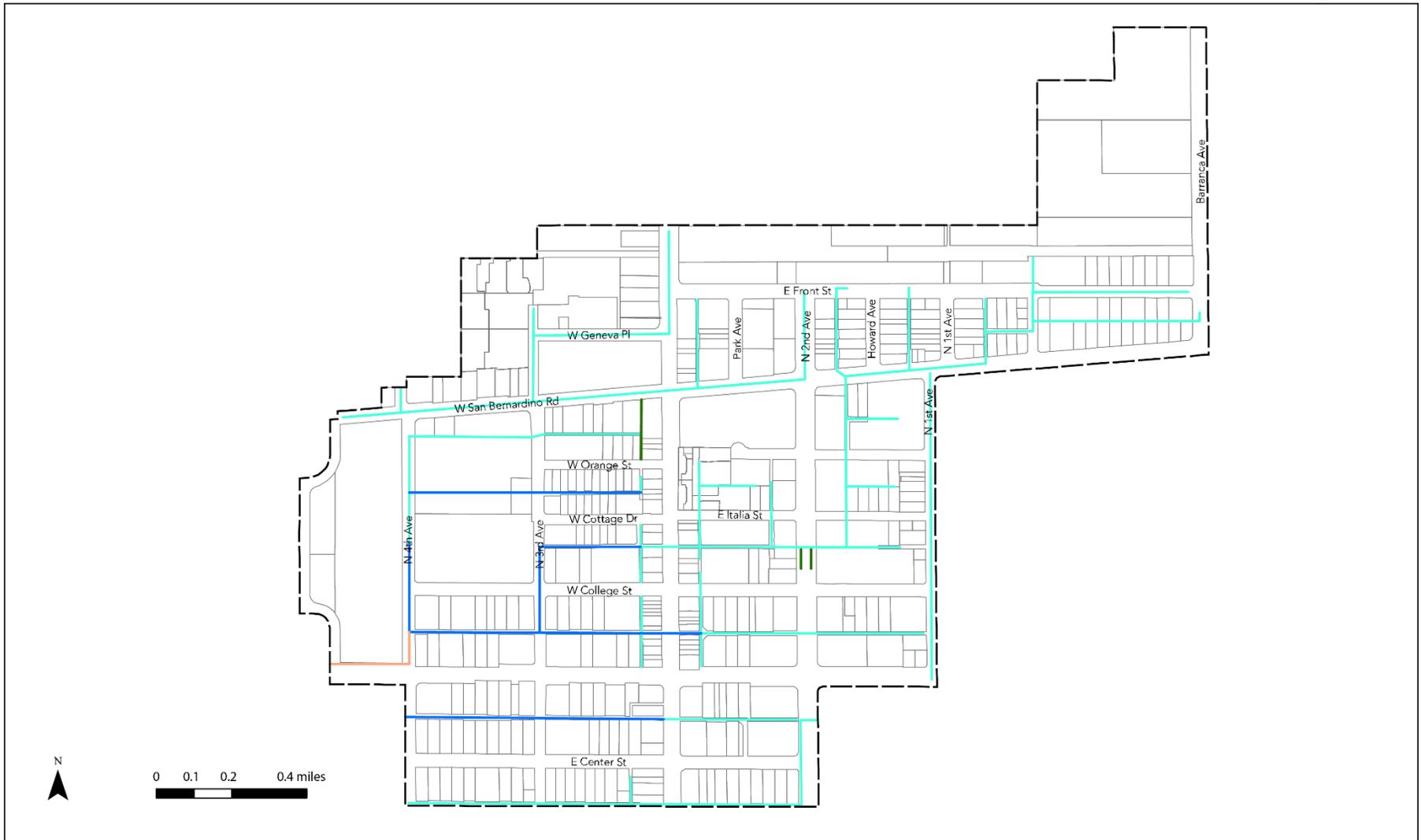
Sanitary Sewer Recommendations

The following improvements are recommended as new development is integrated into the existing Covina Town Center network:

- Facilitate cross-agency review of infrastructure budgets and upcoming projects by the City, the District, and Los Angeles County Department of Public Works.
- Track planned infrastructure improvements and, as feasible, coordinate these with improvements associated with implementation of this Specific Plan to minimize costs, street closures, and disturbances associated with construction.
- Support ongoing monitoring and maintenance of local sewer lines.
- Require restaurants and/or food-related businesses to install grease interceptors and participate in annual inspection program.

Sewer Lines Map

Figure 5-23



LEGEND	
<6" Sewer Pipe	10" Sewer Pipe
8" Sewer Pipe	12" Sewer Pipe

Source: 2014 Covina Sewer System Management Plan

STORM DRAINAGE INFRASTRUCTURE

The **continuous maintenance and improvement of storm water quality is imperative for the protection of public health, wildlife, and watersheds.** Through stormwater management and appropriately designed development, water pollution can be dramatically reduced.

Covina's storm drainage system is largely administered by the Los Angeles County Flood Control District, which manages 500 miles of open channel, 2,800 miles of underground storm drains, and an estimated 120,000 catch basins. The District encompasses approximately 3,000 square miles, 85 cities, and approximately 2.1 million land parcels. The storm drain system represents critical infrastructure in the CTCSP area's highly developed and impervious environment.

The drainage system in the CTCSP area is made up of a network of reinforced concrete pipe storm drains located beneath the public street and alley rights-of-way. Stormwater drainage from individual properties is facilitated by lateral lines that connect to the storm drain system. Catch basins located throughout the Specific Plan area prevent clogging of storm drains by sediment and debris washed off streets and other surfaces. Most catch basins are maintained by the Los Angeles County Flood Control District or the City of Covina.

Water Quality

Low-Impact Development (LID) principles can be applied to manage, reduce, and re-use stormwater runoff whenever possible. These LID elements provide water quality treatment and delay stormwater runoff, while also enhancing the urban landscape. Reducing the amount of stormwater runoff entering underground drainage systems can decrease infrastructure upgrade costs, eliminate potential capacity deficiencies, and mitigate stormwater runoff pollution. Development projects within the Covina Town Center are required to comply with the urban runoff pollution control provisions, which regulates the treatment of stormwater runoff. Development projects are encouraged to incorporate Best Management Practices into their site design to improve local storm water quality and decrease runoff. Examples of stormwater treatment controls/LID techniques incorporated through design include bioretention areas, flow-through planter boxes, vegetated buffer strips, infiltration trenches, green roofs, green streets, permeable pavements, and rainwater harvesting/use.

These elements should be incorporated on individual sites as well as within streetscapes in the public right-of-way. Other BMPs may be approved by the City of Covina or the county-wide program in the future to address National Pollution Discharge Elimination System (NPDES) Permit requirements. Specific LID techniques to be implemented are discussed in Chapter 4.

Stormwater Recommendations

The following improvements are recommended as new development is integrated into the Covina Town Center:

- Encourage stormwater mitigation improvements in both the public and private realms through the development of stormwater planters and green infrastructure, such as bioswales, detention basins, bioretention areas, and flow-through planter boxes.
- Work with developers to promote, approve, and implement designs that include the integration of LID strategies.
- Encourage site designs that maximize permeable surface cover and infiltration potential.
- Encourage outdoor seating areas to be swept or mopped daily to remove debris and prevent bacteria from entering the storm drain system.

Stormwater LID Images



Figure 5-24

- 1 Bulb-out with stormwater LID
- 2 Bioswales



LEGEND

--- Maintained by City of Covina	--- 30" storm drain	--- 48" storm drain
--- Maintained by LACFCO	--- 36" storm drain	--- 51" storm drain
--- 24" storm drain	--- 42" storm drain	--- 54" storm drain
--- 27" storm drain	--- 45" storm drain	

Source: MIG, City of Covina, and LA County Public Works 2018.

DRY UTILITIES INFRASTRUCTURE

The following section describes **the capacity of dry utilities in the Covina Town Center area and potential opportunities for improvements.**

Electricity and Natural Gas

Electrical power in Covina is provided by Southern California Edison (SCE). SCE serves over 14 million people and covers an area of approximately 50,000 square miles across 180 cities. Natural gas is provided by The Gas Company (SoCal Gas), which serves the Covina Town Center as well as 21.4 million customers in more than 500 cities.

Broadband and Telecommunications

Cable television service is provided by Spectrum and Frontier. Telephone service is also provided by Frontier Communications. It is anticipated that one (or both) of these providers would provide cable, internet, and telephone service to the Specific Plan area.

Solid Waste

The City contracts with a private hauler, Athens Services/Covina Disposal, for residential and commercial solid waste and recycling pickup and disposal. All solid waste is disposed of and sent to a landfill. State law requires business and organic recycling, which is provided by hauler.

Dry Utilities Recommendations

The following improvements are recommended as new development is integrated into the Covina Town Center:

- Work with utility companies to determine timelines and phasing programs for new infrastructure to meet future demand.
- Track planned dry utility improvements and, as feasible, coordinate these with improvements associated with implementation of the CTCSP, to minimize costs, street closures, and disturbances associated with construction.



San Bernardino Rd

San Bernardino

OPEN DAILY
MONDAY - SATURDAY
10:00 AM - 6:00 PM

LAST 20¢

CITY OF
San Bernardino
Land

COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 6: IMPLEMENTATION





Chapter 6 Contents

- Overarching Objectives
- Specific Plan Administration
- Non-Conforming Uses and Buildings
- Funding Sources
- Implementation Action Plan

CHAPTER 6: IMPLEMENTATION

IMPLEMENTATION

A coordinated and systematic implementation of the Covina Town Center Specific Plan is essential to accomplish the vision for the area's future.

Chapters 1-5 identify the type of development desired in the Covina Town Center and outline a variety of potential improvements to enliven the area and catalyze new private investment. This implementation chapter provides a set of tools focusing on a variety of financing mechanisms and project-specific funding priorities. This chapter, also, describes the administrative processes to review potential new development projects.

Implementing the CTCSP will require a collaborative effort between the public and private sectors to collectively achieve the overall vision. A significant portion of the Covina Town Center's new investment and construction will need to be made by private sector developers, entrepreneurs, investors, and property owners. The City's responsibility is to offer mechanisms to facilitate private investment while at the same time coordinating and encouraging parallel and complementary investment in the public realm.

Due to a fluctuation in economic conditions and trends, the City may need to periodically revisit and reprioritize the CTCSP's implementation. The tools and funding mechanisms presented here are provided with the understanding that market shifts and varying economic conditions require flexibility to accommodate new development and facilitate additional investment.



OVERARCHING OBJECTIVES

Prioritized implementation of the various forms of development and physical improvements outlined in this Specific Plan will generate additional tax revenue and employment opportunities. The following overarching objectives related to economic development will support the CTCSP's underlying vision and encourage investment in the Covina Town Center area:

- Expand high quality employment opportunities through the increased concentration of local retail, restaurant, office, medical, and industrial spaces.
 - Prioritize land use improvements consistent with community planning objectives, based on economic analysis, and focus efforts to capitalize on current market opportunities.
 - Facilitate new development and business activity and expansion.
 - Increase housing opportunities for commuters who use the Metrolink so they may live and shop in the Covina Town Center area.
 - Take advantage of vacant and underutilized sites to expand residential, mixed-use, commercial, and industrial development.
- Seek and encourage innovative retail and job-creating uses that are responsive to emerging economic, social and demographic changes.
 - Ensure developers build flexible commercial spaces able to respond to variations in the market.
 - Intensify concentration of public improvements to support and encourage private investment within the Covina Town Center.
 - Create long term public/private partnerships to lead the process of physical change synonymous with the objectives of the Specific Plan.
 - Proactively attract and retain new businesses, jobs, and investment.
 - Establish financing mechanisms to implement public improvements and business improvement activities in prioritized areas.
 - Maintain the City's business-friendly environment and continue to offer new and existing businesses an effective and streamlined process.

SPECIFIC PLAN ADMINISTRATION

The CTCSP serves as the implementation tool for the General Plan and establishes the zoning regulations for the Specific Plan area. The CTCSP addresses general provisions, permitted uses, development and design standards, design guidelines, mobility improvements, and infrastructure. All development proposals within the Specific Plan area are subject to the procedures established herein.

Interpretation

In the event of ambiguity or circumstances, not specifically provided in this CTCSP, the Director of Community Development shall interpret the intent of the land use standards. Any person aggrieved by such an interpretation may request an interpretation by the Planning Commission. If this person is aggrieved by the interpretation of the Planning Commission, this person may appeal to the City Council for a final determination.

Severability

If any section, subsection, sentence, clause, phase, or portion of this Specific Plan, or any future amendments or additions, is for any reason held to be invalid or unconstitutional by the decision of any court or competent jurisdiction, such decision shall not affect the validity of the remaining portions of the CTCSP, or any future amendments or additions.

Environmental Review

In conjunction with one another, the CTCSP and CTCSP Environmental Impact Report (EIR) for the CTCSP area may reduce the need for project-specific environmental review in areas that have been analyzed by the EIR, subject to findings that there are no significant changes in conditions and that the project complies with the CTCSP's requirements, and pursuant to CEQA Guidelines Section 15177 and Public Resources Code 21157.1. Certain projects may require additional specific environmental review as necessary, if they do not conform with the CTCSP. This could include targeted studies on one or more identified environmental concerns. The

City will make these determinations and environmental review may be incorporated in the development approval process

Relationship to Zoning Code

Any details, design guidelines and standards not covered by the CTCSP shall be subjected to provisions of the Covina Zoning Code, Title 17, as maybe amended from time to time. Where there is a conflict between the CTCSP Design Guidelines and Standards and Covina Zoning Code, the CTCSP Design Guidelines and Standards shall apply.

Review and Approval Process

All projects proposed within the CTCSP shall substantially conform with the provisions of this Specific Plan. CMC Chapter 17.64 Site Plan Review sets forth the development review requirements, the approval authority and the review process, which shall apply to projects proposed within the Specific Plan area. The time limits for approval and construction, the appeal process, the revisions and revocation process for Site Plan Review are contained in CMC Chapter 17.64. The Director of Community Development or designee shall determine and make findings that the project has complied with CTCSP, prior to project or Site Plan Review approval if the Director is the approval authority.

Specific Plan Amendments and Minor Modifications

Modifications to the text or exhibits of this Specific Plan may be warranted in the future to accommodate unforeseen conditions or events. The City will process revisions in a manner pursuant to the provisions in this section.

Amendments

Proposed changes to this Specific Plan that do meet the criteria for a Minor Modification shall be subject to a formal Specific Plan Amendment process pursuant to the California Government Code Section 65450, et seq. All CTCSP amendments shall be found consistent with the Covina General Plan in compliance with Government Code

Section 65454. The CTCSP may be amended as often as deemed necessary by the Council in compliance with Government Code Section 65453.

Minor Modifications

Minor modifications are modifications to a previously approved development plan or entitlement under the CTCSP. Minor modifications to approved development plans include the following types of project changes:

- Minor modifications to design guidelines for: 1) architectural features and materials; 2) landscape treatments; 3) lighting; or 4) signage.
- Revisions to bike facilities locations and roadway lane and pedestrian passageway widths.
- An increase of less than ten percent of the building footprint.
- A change in the layout of the parking or loading area.
- The relocation of windows or doors on one or two wall surfaces.
- An adjustment in the location of buildings provided the general location of each building is similar to the approved development plan.
- Changes to a conditional use permit that does not require the physical modification of the site.
- Changes to the approved landscaping plant palette.
- Changes in exterior colors or materials.

Use Determinations

An element in the CTCSP vision statement is to allow flexibility of uses to create new active spaces. The goal of the CTCSP is to encourage commercial and retail

uses, such as shared space for retail and food, indoor commercial recreation uses, boutique winery and microbreweries that attract complementary and symbiotic uses for a vibrant lifestyle destination for the Downtown. The Director may determine that a proposed use not listed in the zoning designations (**Table 3-2 Allowable Land Use**) is allowed if all of the following findings are made:

1. Similar use may be allowed.
 - a. The characteristics of, and activities associated with, the proposed use are equivalent to those of one or more of the uses listed in the zoning designations as allowable, and will not involve a higher level of activity or population density than the uses listed in the zoning designations;
 - b. The proposed use will meet the purpose or intent of the zoning designations that is applied to the site; and
 - c. The proposed use will be consistent with the goals and visions of the CTCSP.
2. Applicable standards and permit requirements. When the Director determines that a proposed, but unlisted, use is equivalent to a listed use, the proposed use will be treated in the same manner as the listed use in determining where it is allowed, what permits are required and what other standards and requirements of the CTCSP and Zoning Code shall apply.
3. Commission determination. The Director may request that the Commission make a determination regarding equivalent uses at a public meeting.

Appeals

All appeals pertaining to the CTCSP shall be made pursuant to the zoning code amendment provisions of CMC Chapter 17.80. If the Director is the approval authority, the decision of the Director is final unless an appeal is received within

10 calendar days from the approval date. If the Planning Commission is the approval authority, the Director shall recommend the findings of the project or Site Plan Review to the Planning Commission. The decision of the Planning Commission is final unless an appeal is received within 10 calendar days from the approval date.



NONCONFORMING USES AND BUILDINGS

Any existing use or building that becomes nonconforming as a result of the CTCSP shall comply with all rules and regulations set forth below.

A. Definitions

1. Nonconforming property means any subdivision of land that was lawfully established and in compliance with all applicable ordinances and laws at the time the property was subdivided.
2. Nonconforming building means any structure, building or improvement upon land, other than land itself including sign that was lawfully established and in compliance with all applicable ordinances and laws at the time the structure, building or improvement was established.
3. Nonconforming use means any use of land that was lawfully established and in compliance with all applicable ordinances and laws at the time the use was established.
4. An existing use shall be deemed a “legal nonconforming use” if prior to its establishment the required permits were obtained, prior to the adoption of the CTCSP (i.e. a building permit, conditional use permit, etc.).
5. Structures and uses not having acquired the proper permits shall be considered “illegal” and shall be define merely as “nonconforming” unless otherwise provided in this section.

B. Continuation and Maintenance

1. A use lawfully occupying a structure or a site, that does not conform with the use regulations or the development standards for the zoning designation in which the use is located shall be deemed to be a nonconforming use and may be continued, except as otherwise provided in this chapter.
2. A structure, lawfully occupying a site, that does not conform with the development standards for front yards, rear yards, height, coverage, or distances between structures, for the zoning designation in which the structure is located, shall be deemed to be a non-conforming use and may be continued, except as otherwise provided in this chapter.

3. Routine maintenance and repairs that do not constitute an alteration or addition may be performed on a structure or site when the use is nonconforming, and on a nonconforming structure.

C. Alterations and Additions to Nonconforming Uses and Structures

1. A nonconforming structure, which is nonconforming by reason of its use, shall not be moved, altered or enlarged unless required by law, or the moving, alteration or enlargement will result in the elimination of the nonconformity, except as otherwise provide in this section.
2. A nonconforming use shall not be enlarged, intensified or extended in such a way as to occupy any part of the structure or site or enlarged or another structure or site which it did not occupy at the time it became a nonconforming use or in such a way as to displace any conforming use occupying a structure or site, except as otherwise provide in this section.
3. A nonconforming structure shall not be altered or reconstructed so as to increase the discrepancy between existing conditions and the development standards for front yards, side yards, rear yards, height of structures, or usable open space prescribed in the development standards for the zoning designation in which the structure is located. A nonconforming structure shall not be moved or enlarged unless the new location or enlargement shall conform to the development standards, or usable open space prescribed in the in the development standards for the zoning designation in which the structure is located.
4. A use which fails to meet the development standards of the zoning designation in which it is located shall not be enlarged or extended, or shall not have equipment replaced that results in failure to meet development standards unless the enlargement, extension or replacement will result in elimination of nonconformity with development standards for that zoning designation.

D. Discontinuation of Nonconforming Use

Whenever a nonconforming use has been discontinued, or changed to a conforming use for a continuous period of 180 days or more, the nonconforming use shall not be reestablished, and the structure or site thereafter shall be in conformity with the development standards for the zoning designation in which it is located. Discontinuance of a use shall include cessation of the existing nonconforming use, regardless of intent to resume said nonconforming use.

E. Restoration of Damaged Structure

1. Whenever a structure which does not comply with the standards for front, side, and rear yards; height of structures; distances between structures; and parking facilities as prescribed in the regulations for the zoning designation in which the structure is located, or for the use which does not conform with the regulations for the district in which it is located, is destroyed by fire or other calamity, by act of God, or by the public enemy to the extent of 50% or less, the structure may be restored and the nonconforming use may be resumed, provided that restoration is started within one year and diligently pursued to completion. When the destruction exceeds 50% or the structure is voluntarily razed or is required by law to be razed, the structure shall not be restored except in full conformity with the regulations for the district in which it is located, and the nonconforming use shall not be resumed.
2. The extent of damage or partial destruction shall be based upon the ratio of the estimated cost of restoring the structure to its condition prior to such damage or partial destruction to the estimated cost of duplicating the entire structure, as it existed prior thereto. Estimates for this purpose shall be made by or shall be reviewed and approved by the building official and shall be based on the minimum cost of construction in compliance with the building code.
3. Restoration of a nonconforming use or structure shall require the issuance of a Site Plan Review, CMC Chapter 17.64.

F. Exceptions to Provisions of Nonconforming Uses and Structures

Expansion of a legal nonconforming uses and structures such as an increase in floor area or the site area or an intensification of use may be permitted subject to a Conditional Use Permit. The Planning Commission shall make the following findings to approve the expansion of the legal nonconforming use and structure:

1. The expansion of the nonconforming structure and/or site comply with the Chapter 4: Design Guidelines and Standards, and Chapter 5: Mobility and Infrastructure of the Specific Plan; and
2. The expansion of the nonconforming use does not increase the discrepancy between existing conditions and the standards for front yard, side yards, rear yard, height of structures, distances between structures, and parking facilities as prescribed in the regulations for the zoning designation in which the structure is located.

FUNDING SOURCES

Overview of Financing

A range of funding sources and financing options are available for infrastructure development in California. While various innovative tools and techniques may be available, five primary sources will nearly always form the backbone of any financing program, as follows:

- Development-Based Funding
- Land-Secured Funding and Financing
- City Funding and Financing
- State and Federal Programs
- Private Investment Funding Sources

Development-Based Funding

California jurisdictions have relatively broad authority to impose obligations on new development to help cover the cost of infrastructure and related community benefits. California cities have a long history of obtaining community benefits from real estate development through a variety of mechanisms, including fees, conditions of approval, and development agreements. Development impact fees, usually imposed under the Mitigation Fee Act, are the most common form because they can be applied to “by-right” development projects, assuming appropriate “nexus” requirements are met.

However, when the public sector creates value through up-zoning and infrastructure development, landowners often enjoy a financial gain in the form of higher land value, which is realized when they sell or develop their land. This increase in land value is an unearned financial benefit that accrues to the private sector, though it is generated (and commonly paid for) by tax-payer funded public entities. Community Benefit Incentive Zoning is a particular variety of development-based funding that works to capture some of the unearned financial benefit, as described further below. The CTCSP is structured around Community Benefit Incentive Zoning.

Land-Secured Funding and Financing

California and jurisdictions through the United States frequently use land-secured financing methods to fund local infrastructure or provide services that benefit a particular area (ranging from an entire jurisdiction to sub-areas of all sizes). While increased voting requirements from Proposition 218 have created limitations on these mechanisms, the Mello-Roos Community Facilities District (CFD) remains a relatively flexible and well-used infrastructure finance tool. In addition, the State has recently increased the ease in which jurisdictions can leverage property tax increment through Enhanced Infrastructure Financing Districts.

Community Facilities District

The Mello-Roos Community Facilities Act of 1982 (*authorized by Section 53311 et. seq. of the Government Code*) enables the formation of a Community Facilities District (CFD) by local agencies, with two-thirds voter approval (or landowner

approval when there are fewer than 12 registered voters in the proposed district), for imposing special taxes on property owners. The resulting special tax revenue can be used to fund capital costs or operations and maintenance expenses directly, or they may be used to secure a bond issuance, the proceeds of which are used to fund capital costs. Because the levy is a tax rather than an assessment, the standard for demonstrating the benefit received is lower, thus creating more flexibility. Despite limited use in populated infill areas, CFDs have become the most common form of land-secured financing in California.

Infrastructure Financing Districts and Enhanced Infrastructure Financing Districts

Infrastructure Financing Districts (IFDs) and Enhanced Infrastructure Financing Districts (EIFDs) are forms of Tax Increment Financing (TIF) available to California public entities. Local agencies may establish an IFD or EIFD for a given project or geographic area to capture incremental increases in property tax revenue from future development. In the absence of the IFD or EIFD, this revenue would accrue to the city's General Fund (or other property-taxing entity revenue fund). EIFD funds can be used for project-related infrastructure, including roads and utilities, as well as parks and housing. Unlike prior TIF/Redevelopment law in California, IFDs and EIFDs do not provide access to property tax revenue beyond the local jurisdiction's share.

City Funding and Financing

The City of Covina has some financial resources available to stimulate development and support capital improvement projects, as discussed below. In addition, California law allows for the recovery of costs associated with the preparation of a Specific Plan. The City may develop a process through which the costs incurred to develop the CTCSP will be reimbursed by future development within the Specific Plan area.

Capital Improvements Program

The Covina Capital Improvements Program (CIP) is a comprehensive analysis of needs and available financing for all capital improvements in the City. The CIP is updated every five years and implemented on an annual basis.

Development Impact Fees

A development impact fee is an ordinance-based, one-time charge on new development designed to cover a "proportional-share" of the total capital cost of necessary public infrastructure and facilities. The creation and collection of impact fees are allowed under AB-1600 as codified in California Government Code Section 66000, known as the Mitigation Fee Act. To the extent that required improvements are needed to address both "existing deficiencies" as well as the projected impacts from growth, only the portion of costs attributable to new development can be included in the fee. Consequently, impact fees commonly are only one of many sources used to finance a city's needed infrastructure improvements. Fees can be charged on a jurisdiction-wide basis or for a particular sub-area of the jurisdiction (such as the Covina Town Center Specific Plan area).

General Obligation Bonds

A general obligation bond is a type of municipal bond that is secured by a state or local government's pledge to use legally available resources, most typically including property tax revenues, to repay bond holders. General obligation bonds are restricted to defined capital improvements. Credit rating agencies often consider a general obligation pledge to have very strong credit quality and frequently assign them investment grade ratings. In California, cities must secure a two-thirds voter approval to issue general obligation bonds.

Adopt-a-XX Program (Light, Bench, Gateway Monument, Tree, Trail, etc.)

As a unique method for paying for streetscape or recreation elements, a small project plaque sign could be affixed to the resource element with the name or logo of the local merchant/business/person/entity purchasing or maintaining the resource, element, or fixtures. This program can also be applied to street trees, benches, gateway monuments, light fixtures, paving surfaces, and banners.

Business Improvement District

A BID is a business-driven process, not a City process. A BID can be used to collect funds for improvements such as streetscape improvements, public safety measures, banners, event coordination, marketing, or maintenance. Funds can be collected based on revenue, lot size, or building size, and could be used to make the improvements described in this Specific Plan.

Benefit Assessment Districts

Benefit Assessment Districts are a set of special annual ongoing assessments that function as overrides over and above the existing property tax assessment limitations imposed by Proposition 13 and its various amendments. When a benefit assessment district is adopted, property owners pay an additional assessment on top of their existing property taxes. These annual collections can be used for the ongoing operations and maintenance of landscaping, lighting, streets, and sewers, and other ongoing public costs.

Development Agreement

A Development Agreement can be an effective tool in encouraging public-private partnerships in large-scale projects that are developed over a series of phases. It provides developers a level of certainty about the land use requirements and gives the City an opportunity to advance its local planning policies through comprehensive planning efforts. Benefits to both public and private parties include: greater latitude in approval methods for development as well as flexible local land use; public agencies have more flexibility in enforcing requirements on proposed development projects; and developers have certainty about land use and regulatory requirements.

Public-Private Partnerships

Public-Private Partnerships (PPP) are considered “creative alliances” formed between a government entity and private developers to achieve a common purpose. Other organizations have joined such partnerships – nonprofits, health care providers, community-based organizations, etc. PPP have been effective in the revitalization of downtowns through development projects such as mixed-use, affordable housing, and community facilities. PPP combine the strengths and resources of both the public sector and the private sector by working together.

State and Federal Programs

Local and regional government entities commonly participate in a range of State and federal grant programs, compete for special grants, and partner with other public agencies on strategic infrastructure improvements. These grant programs and cooperative efforts, while primarily focused on maintenance of existing infrastructure, can be managed in a way that supports revitalization and infill development efforts. While the availability and level of funding from these sources are generally difficult to predict, the City should continue to explore and pursue these opportunities as they arise. Potential funding sources include:

Community Development Block Grant (CDBG)

Established in 1974, the Community Development Block Grant (CDBG) is one of the oldest programs of the Department of Housing and Community Development (HUD). The CDBG provides annual grants based on a formula. CDBG funds must provide benefits to low-income or moderate-income individuals, prevent or eliminate slums or blight, or may be used for other emergency community needs, such as related to a natural disaster. CDBG funds can be used for development purposes within low-income or moderate-income census tracts, or, if the development or activity is located outside of a low-income or moderate-income census tract, funds must provide benefits to low-income or moderate-income households. Funding, available on an annual basis, is approximately \$250,000; reductions may be anticipated with the change of federal priorities. The grants

can be used for revitalization projects, infrastructure improvements, low-income housing, and reduction of blight.

Grant Programs

Local and regional government entities commonly participate in a range of State and federal grant programs, compete for special grants, and partner with other public agencies on strategic infrastructure improvements. These grant programs and cooperative efforts, while primarily focus on maintenance of existing infrastructure and development of sustainable healthy practices (bike trails and greenways), can often fund capital improvements such as housing, transit, and infrastructure that support revitalization and infill development efforts.

State Infrastructure Bank (IBank)

The IBank was created in 1994 to finance public infrastructure and private development that promote a healthy climate for jobs, contribute to a strong economy, and improve the quality of life in California communities. The IBank operates pursuant to the Bergeson-Peace Infrastructure and Economic Development Bank Act (Government Code Sections 63000 et seq.). The IBank is administered by the Governor's Office of Business and Economic Development and is governed by a five-member Board of Directors. Since its inception, the IBank has financed more than \$32 billion in infrastructure and economic development projects around the State.

The IBank has broad authority to issue tax-exempt and taxable revenue bonds, provide financing to public agencies, provide credit enhancements, acquire or lease facilities, and leverage state and federal funds. The IBank's current programs include the Infrastructure State Revolving Fund (ISRF) Program, 501(c)(3) Revenue Bond Program, Industrial Development Revenue Bond Program, Exempt Facility Revenue Bond Program and Governmental Bond Program.

The ISRF Program provides very low-interest rate loans up to \$25 million (per applicant) to municipal governments for a wide variety of municipal infrastructure, including infrastructure needed to serve new development. An application is required for these loans, and loans require a stable and reliable source of repayment. If approved, loan repayment can be funded through a

commitment of city general fund revenues or a pledge of a particular revenue source, including a citywide tax, land secured assessment, or special tax levied on a particular area.

California Department of Housing and Community Development (HCD) Loans and Grants

The California Department of Housing and Community Development (HCD) administers more than 20 programs that award loans and grants for the construction, acquisition, rehabilitation, and preservation of affordable rental and ownership housing, infill residential and mixed-use projects, public facilities (including parks and trails), and infrastructure, as well as the development of jobs for lower-income workers. These loans and grants are available to local public agencies, nonprofit and for-profit housing developers, and service providers. In many cases, these organizations then provide funds to individual end users.

Metropolitan Transportation Authority (Metro) Call for Projects

Metro is responsible for allocating discretionary federal, State and local transportation funds to improve all modes of surface transportation. Metro also prepares the Los Angeles County Transportation Improvement Program (TIP). A key component of TIP is the Call for Projects program, a competitive process that distributes discretionary capital transportation funds to regionally-significant projects. Every other year, Metro accepts Call for Projects applications in seven modal categories. Local jurisdictions, transit operators, and other public agencies are eligible to submit applications proposing projects for funding.

development itself and is often derived from CEQA-based mitigation measures.

Private Investment Funding Sources

Private investment is a necessary component of revitalization. There are multiple avenues through which private investment can benefit the Covina Town Center. Some of these sources are outlined below.

Private Financing, Agreements, and Partnerships

Developers commonly fund infrastructure requirements privately, for example virtually all “in-tract” improvements (infrastructure improvements within a subdivision) are privately financed. In some cases, area-serving infrastructure (not fully the responsibility of a particular developer) can be privately financed. These cooperative arrangements are typically structured in development agreements or reimbursement agreements. This upfront infrastructure development may be fully or partially refunded, using subsequently-collected development impact fees, special tax bond proceeds, or other city funding sources. These arrangements tend to be available during times of strong market performance. In weaker markets or locales, it may be difficult to obtain such private financing.

Project Specific Conditions and Exactions

Before the advent of ordinance-based development impact fees, it was common for infrastructure to be funded by the developer through project-specific exactions imposed by the local jurisdiction, including direct payments for or construction of infrastructure required as a condition of subdivision or project approval. While development impact fees have reduced the use of exactions, exactions remain an important part of development-based infrastructure financing as there are often infrastructure requirements of a new project that are not included in the applicable fee programs. Determination of the need for such additional infrastructure is based on “rough proportionality” (i.e., nexus) with the

IMPLEMENTATION ACTION PLAN

The City of Covina will need to undertake a series of specific Policy and Regulatory actions to fully implement the CTCSP. The implementation will require collaboration with local businesses, partner agencies, and the Community Development, Public Works, Engineering, Building and Safety, Code Enforcement, Housing, Community Development Block Grant (CDBG), and Successor Agency Departments. While a significant portion of investment will be determined by private development, the City shall take the lead in coordinated Covina Town Center-wide actions and improvements to enable complete implementation of the CTCSP. The Implementation Action Plan summarizes each action and provides a **priority timeframe, primary responsibilities and partners, approximate costs, and potential funding sources**. This Implementation Action Plan will be used by the City throughout the life of the CTCSP and should be periodically reviewed and updated to reflect conditions as they change over time.

The City will make continued progress towards improving the project area's development readiness through the CTCSP, pursuing individual project opportunities, and supporting on-going re-tenanting and property re-investment activity. Approval of the CTCSP itself represents one of the most important steps toward revitalization because doing so provides the guidance, certainty, and authority needed to induce further private sector investment.

Action	Timeframe	Potential Funding Source	City Department Responsibility	Estimated Cost
1.0 Infrastructure, Mobility, and Sustainability Improvements				
<p>1.1 Water System and Infrastructure</p> <p>Ensure that water system and infrastructure facilities are upgraded, as needed, to support future development. Replace aging water lines, as needed. Require developers to provide engineering studies to confirm sufficiency or required improvements.</p>	Short Range	Private	Public Works	\$-\$\$\$
<p>1.2 Water Conservation</p> <p>Continue to pursue innovated efforts to reduce water consumption through conservation programs and initiatives for both residents and businesses, in accordance with Covina’s Water Conservation Program.</p>	Short Range	General Fund	Public Works	\$
<p>1.3 Recycling and Composting</p> <p>Continue to encourage solid waste, recycling, and composting efforts to move toward minimizing waste sent to landfills and reducing solid waste disposed per capita.</p>	Short Range	General Fund	Public Works, Community Development	\$
<p>1.4 Sewer System and Infrastructure</p> <p>Ensure that existing sewer system is upgraded, as needed, to support future development. Replace aging sewer lines, as necessary and feasible, when resurfacing streets.</p> <p>Require developers to provide engineering studies to confirm sufficiency or identify required improvements.</p>	Short Range	Private	Public Works	\$-\$\$\$\$
<p>1.5 Parkway Landscape and Streetscape Improvements</p> <p>Develop and implement parkway and streetscape landscape guidelines (tree palette, flora and turf palette), streetscape furniture guidelines, and permeable paving guidelines supporting design standards and guidelines established in Chapter 3: Land Use and Zoning. Make guidelines available to</p>	Mid-Range	General Fund, CIP	Public Works, Parks and Recreation	\$\$

Action	Timeframe	Potential Funding Source	City Department Responsibility	Estimated Cost
<p>development applicants, Covina Town Center <i>property owners, and the public.</i></p> <p><i>Improvements include but are not limited to:</i></p> <ul style="list-style-type: none"> • Street trees; • Health corridor amenities; • Library plaza amenities; • 2nd Avenue improvements; • Front Street improvements; • Parklets; • Curb extensions and enhanced/additional marked crosswalks; • Parkway planting; • Sidewalk pavement repair, where needed; • Enhanced sidewalk lighting; • Sidewalk ramps and audible traffic controls comply with ADA requirements; • Directional and informational wayfinding and pedestrian signs; and • Streetscape furniture (benches, shade structures, bus shelters, etc.); <p>Property owner information about their responsibility for street tree maintenance. Identify standard improvements to be accomplished to ensure adequate tree irrigation and health root growth.</p>				
<p>1.6 Gateways</p> <p>Provide entry/gateway signs, landscaping, and lighting to mark and enhance gateways to the Covina Town Center. Gateway amenities should be located on:</p> <ul style="list-style-type: none"> • North Citrus Avenue/East Front Street • Citrus Avenue/Badillo Street • North 4th Avenue/West San Bernardino Road • 4th Avenue/West Badillo Street • Barranca Avenue/East Front Street • Barranca Avenue/East San Bernardino Road 	Mid-Range	General Fund, CIP	Public Works	\$\$-\$\$\$

Action	Timeframe	Potential Funding Source	City Department Responsibility	Estimated Cost
<ul style="list-style-type: none"> • 1st Avenue/East Badillo Street • Gateway entries can be incorporated into public plazas/courtyards. 				
<p>1.7 Metrolink Covina Station Concept</p> <p>Design and construct a series of roadway improvements to North Citrus Avenue and East Front Street as identified in Chapter 5: Mobility and Infrastructure.</p>	Mid-Range	General Fund, CIP	Public Works	
<p>1.8 Transit Center Plans</p> <p>Develop a transit center plan that enhances wayfinding to the Covina Town Center, and enhances streetscape amenities (benches, shade structures, drinking faucets, etc.).</p>	Mid-Range	General Fund	Community Development, Public Works,	\$\$-\$\$\$
<p>1.9 Bicycle and Pedestrian Improvements</p> <p>Design and implement bicycle and pedestrian improvements as described in Chapter 4: Design Guidelines and Standards and Chapter 5: Mobility and Infrastructure.</p>	Mid-Range	General Fund	Community Development, Public Works	\$\$-\$\$\$
<p>1.10 Roadway Modifications</p> <p>Design and implement roadway modifications as described in Chapter 4: Design Guidelines and Standards and Chapter 5: Mobility and Infrastructure.</p>	Mid-Range	General Fund	Community Development, Public Works	\$\$-\$\$\$
2.0 Economic Development Programs				
<p>2.1 Business Improvement District</p> <p>Expand the Covina Downtown Association to include all of the Covina Town Center area or create a Business Improvement District (BID), if initiated and requested by local property owners or businesses. BIDs seek to add specific benefits within a selected area. BIDs are financed through special assessments placed on commercial or industrial property within the designated district. After petitioning the City to form a BID, passage requires</p>	Short Range	General Fund	Administration	\$

Action	Timeframe	Potential Funding Source	City Department Responsibility	Estimated Cost
<p>majority approval by affected property owners. Once formed, BIDs are governed by a board of directors who are elected by property owners in the district rather than by residents.</p> <p>A BID could play a crucial role in the economic enhancement of the Specific Plan area in addition to providing funding for physical improvements. Some of the money collected from the BID could be earmarked for public art, gateway and wayfinding, event coordination, marketing, or maintenance. A BID could also be used by participants to coordinate store hours, merchandise, and create promotions and advertisements.</p>				
<p>2.2 Business Assistance Program</p> <p>Work with residents, start-ups, or Covina Town Center businesses to seek and obtain an Economic Development Loan/Grant. The Economic Development Loan/Grant program provides financial assistance to new and existing businesses to start or expand their businesses, purchase and install equipment or machinery, improve their physical space, or make exterior improvements.</p>	Ongoing	CDBG	Administration	\$
<p>2.3 Stimulate Vitalization</p> <p>Coordinate with property owners, community groups, Emanate Health Inter-Valley Community Hospital to establish a program of events including farmers' markets, vendor fairs, live music, etc. to help brand and revitalize the Covina Town Center.</p>	Ongoing	General Fund	Community Development, Parks and Recreation	\$\$
3.0 Administrative				
<p>3.1 Specific Plan Adoption and Environmental Documentation Certification</p> <p>Adopt the Covina Town Center Specific Plan and certify the Covina Town Center Specific Plan Environmental Impact Report</p>	Immediate	General Fund	Community Development	\$
<p>3.2 General Plan and Land Use Map Amendment</p>	Immediate	General Fund	Community Development	\$

Action	Timeframe	Potential Funding Source	City Department Responsibility	Estimated Cost
Amend the City's General Plan to be consistent with the adopted Covina Town Center Specific Plan land uses, density/intensity standards, policy direction, and map.				
3.3 Zoning Code and Zoning Map Amendment Amend the Covina Zoning Code and Zoning Map to be consistent with the adopted Covina Town Center Specific Plan.	Immediate	General Fund	Community Development	\$
3.4 Detailed Infrastructure Financing Plan Complete cost estimates for all proposed public improvements and identify funding mechanisms and phasing to support new infrastructure improvements within the Covina Town Center.	Short-Range	General Fund	Public Works	\$
3.5 Public Arts Guidelines Develop guidelines for the design (including acceptable materials), location, and installation of public art on private property and within the public realm.	Short-Range	General Fund	Community Development	\$
3.6 Ongoing Funding Mechanism Include any operations and maintenance-related financing actions in conditions of approval or Development Agreement terms for new development projects, as applicable.	Short-Range	General Fund	Community Development, Administration	\$
3.7 Economic Development Plan's Strategic Attraction Business Area Develop an Economic Development Plan/Strategic Attraction Business Plan. Place key mixed-use, commercial, and industrial opportunity sites (to be identified) on the City's Strategic Attraction Business list.	Ongoing	General Fund	Community Development	\$

Action	Timeframe	Potential Funding Source	City Department Responsibility	Estimated Cost
<p>3.8 Flexible Business Operations and Regulations</p> <p>Implement flexible administrative procedures and use regulations that allow businesses in the Covina Town Center to make operational changes with minimal discretionary City approval, recognizing that business operations need to adapt to market conditions and changes in technology.</p>	Short Range	General Fund	Community Development	\$
<p>3.9 Capital Improvement Program Integration</p> <p>Identify applicable capital improvements from this Specific Plan in the Capital Improvement Program (CIP) and update periodically.</p>	Short Range	General Fund	Community Development	\$
<p>3.10 Metro Coordination</p> <p>Coordinate and collaborate with the Metro and the City of Covina to ensure local improvements are constructed and maintained in a timely manner.</p>	Ongoing	General Fund	Community Development	\$
Abbreviations: CIP – Capital Improvement Program, CDBG – Community Development Block Grant				

COVINA TOWN CENTER SPECIFIC PLAN

CHAPTER 7: GLOSSARY





Chapter 7 Contents

- Eating, Entertainment, and Printing Establishments
- Industrial
- Institutions and Public Facilities
- Medical-Related Uses
- Mixed Uses
- Recreation and Public Assembly Uses
- Residential
- Retail Uses
- Service Uses
- Visitor Accommodations
- Floor Area (Gross)

CHAPTER 7: GLOSSARY

GLOSSARY

Land use definitions included within the City of Covina Municipal Code (CMC) Title 17 are, hereby, incorporated by reference.

1.0 EATING, ENTERTAINMENT, AND DRINKING ESTABLISHMENTS

1.1 Alcohol Sales, On-Site with or without Food Service

Any establishment in which alcoholic beverages are sold, served, or given away for consumption on site whether or not accompanied by the sale, dispensing or consumption of food. Includes bona fide or full-service restaurants, bars, lounges taverns, brew-pubs, food to go/take out establishments and other uses similar in nature and operation. Excludes establishments with live entertainment or dancing. The facility shall not exceed 2,000 square feet. (Ord. 1642 § 3, 1987.)

1.2 Accessory Use

A use incidental, appropriate, subordinate, and devoted exclusively to the main use of the lot or building.

1.3 Ancillary Use

Permitted land uses that are secondary and complementary to the principal use, but not accessory. An example of an ancillary use is food or retail sales associated with a microbrewery that only serve the principal brewery use.

1.4 Commercial Kitchen

An establishment where food and beverages are prepared for off-site consumption. Typical use is a catering facility.

1.5 Drive-Through or Drive-Up Facilities

An establishment that sells products or provides services to occupants in vehicles, including drive-in or drive-up windows and drive-through services. Examples include fast food restaurants, banks, and pharmacies. Does not include facilities in which an online/phone order is picked up in a stationary retail business without use of a drive-in service.

1.6 Entertainment and Performance Establishments

“Entertainment and performance establishment” means any building, structure, or premises where entertainment occurs, consistent with CMC 5.28: Entertainment.

1.7 Live Entertainment

“Live entertainment” means every form of live performance – whether a single event, a series of events, or an ongoing activity or business, occurring alone or as a part of another business, to which persons (including patrons and/or guests) are invited or allowed to watch, listen, or participate or that is conducted for the purposes of holding the attention of, gaining the attention of, or diverting or amusing guests or patrons, whether or not they are compensated for the performance. Live entertainment shall include, but shall not be limited to:

- A. Presentations by single or multiple performers, such as hypnotists, pantomimes, comedians, musical song or dance acts (including karaoke performances), plays, concerts, any type of contest; sporting events, exhibitions, carnivals, rodeos, or circus acts; demonstrations of talent; shows, revues; and any other similar activities.
- B. Presentation of recorded music played on equipment that is operated by an agent or contractor of the entertainment establishment, commonly known as a “disc jockey” or “DJ.”
- C. Dancing to live or recorded music (whether by performers, patrons, or guests).

The provisions of CMC Chapter 5.28 shall apply.

1.8 Indoor Recreation Facilities (added 4-18-23)

Indoor recreation Facility conducted wholly within an enclosed building and may include ancillary uses such as café or restaurants. Examples are: archery and shooting ranges, pool and billiard parlors, escape rooms, bowling centers, children’s indoor play space, amusement arcades, virtual reality entertainment, and similar indoor recreational uses. Comply with CMC Chapter 17.62 for on-site alcohol sales, and CMC Chapter 5.28 for entertainment.

1.9 Outdoor Recreation Facilities (added 4-18-23)

Outdoor recreation Facility means an area designed for active recreation, whether publicly or privately owned, including, but not limited to, tennis courts, swimming pools, tennis, badminton, and sport courts for pickleball or paddleball. Outdoor recreation facility may include ancillary uses such as pro-shop, café, clubhouse supporting the recreation facilities. Comply with CMC Chapter 17.62 for on-site alcohol sales.

1.10 Restaurant, Full-Service, Food To Go/Take-out, or Fast Food Ancillary Use (See CMC 17.04.100 for additional standards)

Restaurant, Full Service: A restaurant where customers purchase food and beverages and consume the food and beverages on the premises. Typical characteristics of a full-service restaurant include table service, payment after consumption, and serving food and beverages in non-disposable containers. A full-service restaurant may include incidental or occasional takeout sales.

Restaurant, Food To Go and Take-out: A restaurant or other such establishment designed as a take-out facility in which no food is consumed on the premises and no permanent provisions are made for that purpose. Provisions for deliveries, catering, drive-through or walk-up facilities are permitted.

Restaurant, Fast Food and/or QUICK SERVICE: A restaurant where customers purchase food and beverages and either consume the food and beverages on the premises or take the food and beverages off the premises. Typical characteristics of a fast food restaurant include: the purchase of food and beverages at a walk-up window or counter; no table service; payment for food and beverages before consumption; and the packaging of food and beverages in disposable containers.

1.11 Restaurant, Outdoor Dining

An adjacent, outside area by a food or beverage establishment for the same eating and drinking activities that occur within the establishment. The outdoor dining area may be located in a public right-of-way, subject to review and approval of outdoor dining permit and encroachment permit.

1.12 Shared Space Food/Retail Facility (Food Halls)

A facility consisting primarily of three or more lease areas within an enclosed building that have a shared entrance/lobby area where the primary uses are food related, but may include small retail venues, shared eating areas, and limited live entertainment (no dancing). Excludes indoor/outdoor swap meet type uses.

2.0 INDUSTRIAL (revised 9-20-2022)

2.1 Alcohol Production and On-Site Tasting

An establishment which produces ales, beers, meads, hard ciders, wine, liquor and/or similar beverages on-site. Establishments may also serve beverages on-site and sell beverages for off-site consumption in keeping with the regulations of the Alcohol Beverage Control (ABC) and Bureau of Alcohol, Tobacco, and Firearms (ATF). Includes boutique winery, brewery and distilled alcohol production.

2.2 Industrial, Light

A use engaged in the manufacture, predominately from previously prepared materials, of finished products or parts, including processing, fabrication, assembly, treatment packaging, and incidental storage and hazardous waste management facility for the producer's waste only. Includes the incidental wholesale and/or direct retail sale to consumers of only those goods produced on-site. Includes incidental General Office uses associated with the on-site use. Examples of light industrial uses include but are not limited to the manufacture of electronic instruments, preparation of food products, and manufacture of crafts, art, sculpture, stained glass, jewelry, apparel, furniture, and similar items.

2.3 Outdoor Storage

The storage of various materials (includes any goods, material, merchandise or vehicles in the same place for more than 72-hours) outside of a structure incidental to a principal use. Does not include the keeping of building materials reasonably required for construction work on the premises pursuant to a valid and current Building Permit issued by the City.

2.4 Research and Development

Establishments engaged in study, testing, design, analysis and experimental development and testing of physical products, processes or services, including incidental manufacturing of products or provisions of services to others. This process does not include the general or mass production of the product. Includes the incidental wholesale and/or direct retail sale to consumers of only those goods produced on-site. Includes incidental General Office uses associated with the on-site use. Examples of research and development firms include, but are not limited to electronic hardware research firms and medical testing and analysis. Excludes medical clinics (see Medical Clinic). Excludes manufacturing, except of prototypes (see Industrial Light). Excludes software testing uses (see General Office).

2.4 Indoor Food Production (Urban Agriculture, Vertical Farms)

A technology driven and sustainable form of indoor food production. Urban agriculture and indoor food production typically use one of the three technological methods: hydroponics, aeroponics or aquaponics, and depend on artificial lighting. Such establishments may include rooftop greenhouses, vertical plant factories, vertical farms inside shipping containers. Examples of indoor food production are vegetables and fruits. Cannabis cultivation is strictly prohibited.

3.0 INSTITUTIONS AND PUBLIC FACILITIES

3.1 Cultural/Civic Facilities (Non-Governmental)

Civic Institution: A building or structure owned, operated or occupied by a non-governmental public or quasi-public unit, body, agency or entity.

Cultural Institution: Any facility such as a museum or library that displays or preserves objects of community or cultural interest relating to one or more of the arts or sciences. This use includes libraries, museums, and art galleries.

3.2 Government Facilities

A building or structure owned, operated or occupied by a governmental agency to provide a governmental service to the public.

3.3 Open/Public Space, Usable or Improved

Outdoor space that serves a recreational function or provides visual relief from the building mass. Examples of these uses include public or private plazas, parklets, greenways, and paseos.

3.4 Parking Facilities

A public or private space dedicated to accommodating any parking and loading space/stalls, loading area, backup area, driveways, and aisles. Also includes:

- A. Joint use parking: The use of a single parking facility by several related uses occupying the same or adjacent parcels. For example, the use of a single parking facility by tenants of a shopping center
- B. Shared Parking: The use of a single parking facility by two distinctly different uses with distinctly different hours of operation such that the shared use of the facility can be accomplished without limiting the ability of one use to occupy the facility to the detriment of the other.
- C. Parking structure: A structure that is designed specifically for automobile parking and where there are a number of floors or levels on which automobiles park.

3.5 Parks and Recreation Facilities

Public parks, play lots, playgrounds, dog parks, and athletic fields for non-commercial neighborhood or community use, including sports courts. May include passive outdoor recreation areas that also may be located in conservation areas and/or qualify as "open space." Does not include the same facilities that are privately owned or commercial facilities ("Entertainment and Performance Establishments").

3.6 Schools (Public and Private)

Schools, Public and Private. A publicly-funded or private academic educational institution, including boarding schools; colleges and universities; college and university extensions; elementary, middle/junior, and high schools; charter schools; military academies; and businesses providing instruction in arts, crafts, and languages. This definition does not include Trade and Vocational Schools, Tutoring and Education Centers, or non-tuition part-time instruction at places of religious assembly.

3.7 Trade and Vocational Schools

A public or private school in which students are taught specific skills needed to perform a particular job such as real estate, business and secretarial, electronics, automotive and aircraft, medical and dental, and similar commercial establishments.

3.8 Utilities

All lines and facilities owned and/or operated by a licensed provider and related to the provision, distribution, collection, transmission, or disposal of water, storm drainage, sanitary sewage, oil, gas, electricity, information, telecommunications, telephone cable, and similar services. Includes facilities for the generation of electricity.

4.0 MEDICAL-RELATED USES

4.1 Hospital

A facility providing medical, psychiatric, or surgical services for sick or injured persons primarily on an in-patient basis, and including ancillary facilities for outpatient and emergency treatment, diagnostic services, training, research, administration, and services to patients, employees, or visitors.

4.2 Laboratories, Medical or Dental

Medical Laboratory: A use of land for the processing, testing, measurement and analysis of medical samples obtained from humans and/or animals, in which neither examination nor treatment of humans or animals is provided.

Dental Laboratory: A use of land for the manufacture of dentures and related dental products, in which patients are neither examined nor treated on the premises. Manufacture of dental retail products, such as toothpaste or toothbrushes, is excluded from this use.

4.3 Medical and Dental Office

An office or health facility providing health services including, without limitation, preventative and rehabilitation treatment, diagnostic services, testing and analysis. This use includes offices providing medical, dental, surgical, rehabilitation, podiatric, optometric, chiropractic and psychiatric services, and medical or dental laboratories incidental to these offices and supportive of on-site patient services but exclude inpatient services and overnight accommodation.

4.4 Medical Clinic

A facility providing medical, psychiatric, or surgical service for sick or injured persons exclusively on an outpatient basis including emergency treatment, diagnostic services, administration, and related services to patients who are not lodged overnight. Services may be available without a prior appointment. This classification includes licensed facilities offering substance abuse treatment, blood banks and plasma centers, surgical centers and emergency medical services offered exclusively on an out-patient basis. This classification does not include private medical and dental offices that typically require appointments and are usually smaller scale.

4.5 Mental Health Counseling Center

An office where individuals, families, and groups seek mental health and wellness services.

4.6 Pharmacy

A place where prescription drugs are dispensed. Does not include medical marijuana dispensaries or any establishment where marijuana is bought and consumed.

4.7 Surgical Center

A medical facility where specialty medical and surgical procedures are performed by qualified medical professionals, but where there is no overnight accommodation of the patient.

4.8 Veterinary Services (Animal Hospital/Clinic)

Establishments where household animals receive medical and surgical treatment and may be temporarily boarded in association with such medical or surgical treatment.

4.9 Veterinary Services with Boarding and/or Day Care

Establishments where household animals receive medical and surgical treatment, and which offer boarding or animal day care services and facilities not associated with medical or surgical treatment.

5.0 MIXED USES

5.1 Mixed Use

Development that integrates compatible land uses including commercial, office, retail, and residential uses.

Horizontal Mixed Use: The uses are placed next to each other on the same parcel, such as an apartment building that is adjacent to offices, restaurant, or retail shops.

Vertical Mixed use: Different uses inhabit the same building and sit atop one another, such as residential or office uses over ground floor retail.

6.0 RECREATION AND PUBLIC ASSEMBLY USES

6.1 Assembly/Meeting Facilities – Private

A facility specifically designed and used to accommodate the gathering of person for private assembly and meetings. Examples of these uses include:

- banquet rooms
- churches/places of religious assembly
- private auditoriums
- conference/convention facilities
- meeting halls for clubs and other membership organizations

This definition includes functionally related internal facilities (i.e., kitchens, multi-purpose rooms, storage, etc.). Associated uses (i.e., day care centers or full-time or part-time schools) may be allowed as incidental uses to the primary use.

6.2 Commercial Recreation Facility - Indoors

Recreational facilities where visitors are participant actors rather than spectators. Examples include indoor facilities such as ice- or roller-skating rinks; trampoline and bounce house establishments; bowling alleys; and electronic game and amusement centers (escape rooms). This classification may include snack bars and other incidental food and beverage services to patrons. Bars or restaurants with alcohol sales shall be treated as a separate use and shall be regulated accordingly, even when operated in conjunction with the entertainment and recreation use.

6.3 Commercial Recreation Facility - Outdoors

Recreational facilities where visitors are participant actors rather than spectators. Examples include outdoor facilities such as batting cages, miniature golf course, ice- or roller-skating rinks; trampoline establishments. This classification may include snack bars and other incidental food and beverage services to patrons. Bars or restaurants with alcohol sales shall be treated as a separate use and shall be regulated accordingly, even when operated in conjunction with the entertainment and recreation use.

6.4 Special/Temporary Events

Any activity of limited duration that is permitted through the issuance of a permit pursuant to Chapter 5.08 CMC. (Ord. 04-1910 § 1, 2004). This definition includes carnivals, farmers' markets, seasonal sales, and promotional events.

7.0 RESIDENTIAL

7.1 Live/Work Unit

A unit that combines a work space and incidental residential occupancy occupied and used by a single household. Live/work units have been constructed for such use or converted from commercial or industrial use and structurally modified to accommodate residential occupancy and work activity in compliance with the California Building Code. The working space is reserved for and regularly used by one or more occupants of the unit. Living space includes, but is not limited to, a sleeping area, a food preparation area with reasonable work space, and a full bathroom including bathing and sanitary facilities which satisfy the provisions of applicable codes. Live/work units can include renter-occupant and/or owner-occupant.

7.2 Multiple-Family Dwelling

A structure or portion thereof containing three or more dwelling units designed for the independent occupancy of three or more households. Development types include apartments, townhomes, and residential condominiums. **(Revised 9/1/2020, Resolution CC 20-80)**

7.3 Shared Space (Co-Living)

An "umbrella term" for different types of co-housing, which can be loosely defined as a home where two or more people live together who are not related. Co-housing typically means an intentional community created by and run by residents. Co-living encompasses shared accommodations initiated by an external agent, such as a developer or nonprofit.

7.4 Single-Family Dwelling

A detached structure containing no more than one dwelling unit which, regardless of form of ownership, is designed and/or used to house not more than one household.

7.5 Single Room Occupancy Residential (SRO)

A building or buildings constructed or converted for residential living consisting of one-room dwelling units, where each unit is occupied by a single individual or two persons living together as a domestic unit, and where the living and sleeping spaces are combined. A unit that contains both a bathroom and kitchen shall be considered a studio unit and not a Single-Room Occupancy Unit.

7.6 Small Group Home/Licensed Facilities (6 or fewer persons)

Any residential care facility licensed by the State of California for occupation by six or fewer persons. See definition under Title 22 (Social Security) in the California Code of Regulations (Section 80001[g]).

7.7 Two-Family Dwelling (Duplex)

A building containing two complete dwelling units designed for the independent occupancy of two households.

8.0 RETAIL USES

8.1 Antique Store/Shop

A retail business, principally offering objects for sale to the general public, which are old as compared to the present age or time, which are of a bygone style or vintage, and which were crafted or manufactured at a much earlier period than the present time; and which, because of the passage of time, have taken on an intrinsic value greater than their original value. For purposes of this title, “antique shop” does not include the sale of secondhand merchandise. See CMC 17.040.576: Thrift shops, secondhand and used merchandise stores.

8.2 Arts and Crafts Sales and Galleries

Uses involved in the exhibit, sales, public education, manufacture, and distribution of all manner of arts, crafts, sculpture, and the like. The floor area devoted to the manufacture of arts, crafts, sculpture, and the like shall not exceed the floor area devoted to exhibits and/or sales.

8.3 Grocery Store, Small (less than 10,000 Sq. Ft.)

A self-service retail or wholesale store that primarily sells food.

8.4 Grocery Store, Large (Supermarket) (10,000 Sq. Ft. +)

A self-service grocery and associated consumer goods store divided into departments and also offering prepared foods and food service. May include secondary uses within the store for visitor convenience, such as banking services, retail sales of non-food items, and a pharmacy.

8.5 Retail Store (less than 10,000 Sq. Ft.)

A store with retail sale or rental of merchandise not specifically listed under another use classification. This classification includes retail establishments including but not limited to department stores, clothing stores, furniture stores, pet supply stores, hardware stores, and businesses retailing miscellaneous goods. Retail sales may be combined with other services such as office machine, computer, electronics, and similar small item repairs. Does not include adult-oriented businesses, swap meet, pawn shop, or secondhand stores.

8.6 Retail Store (10,000 Sq. Ft.+)

A store with retail sale or rental of merchandise not specifically listed under another use classification. This classification includes retail establishments including but not limited to department stores, clothing stores, furniture stores, pet supply stores, hardware stores, and businesses retailing miscellaneous goods. Retail sales may be combined with other services such as office machine, computer, electronics, and similar small item repairs. Does not include adult-oriented businesses, swap meet, pawn shop, or secondhand stores.

8.7 Shared Space (Retail)

A retail establishment in which one or more companies share retail space within a single structure; for example, a coffee-store sharing space with a book store. Shared space retailers may display separate logos on storefront.

8.8 Specialty Store

Retail establishments that focus on selling a particular product range and associated items. Specialty stores maintain considerable depth in the type of product that they specialize in selling and provide higher service quality and expert guidance to shoppers. This classification includes but is not limited to electronics/video game stores, specialty food stores, new and used book or record/music stores. This classification does not include swap meets, pawn shops, thrift shops, secondhand and other used merchandise stores.

8.9 Smoke Shop and Tobacco Store

A retail establishment that primarily sells tobacco products, tobacco paraphernalia, electronic cigarette and vaping devices, and/or electronic cigarette and vaping accessories. Excludes grocery store, supermarket, convenience store or similar retail use that only sells conventional cigars, cigarettes or tobacco as an ancillary sale.

9.0 SERVICE USES (revised 9-20-2022)

9.1 Animal Boarding/Kennel

The commercial provision of shelter and care for dogs, cats, other household animals, and horses (where allowed), including activities associated with such shelter and care (e.g., feeding, exercising, grooming, and incidental medical care).

9.2 Bank/Financial Institution

A bank, savings and loan, credit union, or other financial institution that provides retail banking services to individuals and businesses. These uses include only those institutions engaged in the on-site circulation of cash money. This does not include check cashing shops/payday loans.

9.3 Business and Consumer Support Service

Business and consumer support services include establishments primarily providing consumers and businesses with services, including maintenance, repair, testing, and rentals. Examples of these uses include: computer and office/business equipment services, on-line ticket services, shoe repair shops, and tailor and seamstress services.

9.4 Barber shops, Beauty Salons including Hair, Nails and Facial (neck up only)

Establishments primarily engaged in one or more of the following: (1) cutting, trimming, shampooing, coloring, waving, or styling hair; (2) providing facials; and (3) applying makeup (except permanent makeup). Excludes stand-alone massage establishments, which are prohibited. Medical skin care services (e.g., cosmetic surgery, dermatology) are classified as Medical.

9.5 Health/Fitness Facility

An indoor facility where passive or active exercises and related activities are performed using minimal muscle-building equipment or apparatus for the purpose of physical fitness, improved circulation or flexibility, and/or weight control. Examples of uses include gym, Pilates, personal training, physical therapy, dance, yoga and martial arts studios, health spa, and other use that are similar in nature and operation. Excludes stand-alone massage establishments, which are prohibited.

9.6 Office, General (revised 4-18-23)

An establishment providing direct, "over-the-counter" services to consumers (e.g., insurance agencies, real estate offices, travel agencies, utility company offices, etc.) and office-type facilities occupied by businesses providing professional services and/or engaged in the production of intellectual property, such as accounting, architectural, computer software design, engineering, graphic design, interior design, investment, co-working spaces, network mixer spaces, conference spaces, maker spaces, and legal offices, excluding banks and financial institutions (see Bank/Financial Institution).

9.7 Personal Services (General)

Establishments that provide recurrently needed services of a personal nature (*i.e., those provided to individuals rather than businesses*). Examples of these uses include:

- acupuncture and acupressure
- standalone locksmith shops
- Stand-alone tanning Salons, body waxing salons
- pet care (except veterinary) services such as boarding, grooming, sitting, walking, and training pets.
- Excludes stand-alone massage establishments, which are prohibited.

9.8 Studio – Art, Dance, Music

Small-scale instructional facilities or a small practice space for the individual artist, musician, or any individual practitioner of the activities defined here, typically accommodating one group of students at a time, in no more than one instructional space. Examples include: individual and group instruction and training in the arts, production rehearsal, photography and the processing of photographs produced only by users of the studio facilities. Also includes production studios for individual filmmakers, musicians, painters, sculptors, photographers, and other artists. These uses may also include accessory retail sales of products related to the services provided. Does not include dance, yoga, gymnastics, and martial arts studios (see Health/Fitness Facility).

9.9 Specialty Cigar Lounge with On-site Alcohol

A specialty cigar lounge is an establishment that caters to patrons who smoke cigars. Such establishment offers cigar products only and does not offer other types of tobacco products for on-site smoking. Such establishments shall have at least 1,500 square feet of gross floor area with built-in humidors. Such establishment is characterized as a quiet, library style lounge where patrons come to relax and unwind in a serene and relaxing environment, instead of a location where they need to make a purchase and immediately leave. Typically there may be soft music, may offer a variety of newspapers, magazines, and may have a fully stocked bar to serve drinks and food (with or without on-site preparation).

10.0 VISITOR ACCOMMODATIONS

10.1 Airbnb/VRBO

Privately owned single room, apartment, or home rented to short term guests by property owner. Where the property owner/host is reviewed by the guest and guests are reviewed by host. Reservations and payment are made through a third party organization.

10.2 Bed & Breakfast Inn or Lodge

A single-family, two-family, or multiple dwelling (whether or not located on the first floor) in which paying guests are lodged on an overnight transient basis, with meals served in connection with their lodging. Such inns or lodges shall contain greater than six lodging units. Meals shall only be served to residents or guests of the inn/lodge and not to the general public. There shall be no separate or additional kitchen facilities for guests. The period for consecutive overnight lodging for any guest shall not exceed 30 calendar days.

10.3 Hotel

A commercial establishment offering overnight visitor accommodations, but not providing room rentals on an hourly basis. A hotel may include ancillary facilities such as common meeting rooms, dining facilities, and guest amenities.

10.4 Motel

A commercial establishment offering overnight visitor accommodations, but not providing room rentals on an hourly basis typically featuring a series of rooms whose entrances are immediately adjacent to a parking lot. A motel may include ancillary facilities such as common meeting rooms, dining facilities, and guest amenities.

11.0 FLOOR AREA (GROSS)

The area included within the surrounding exterior walls of a building or portion thereof, exclusive of garages. The floor area of a building, or portion thereof, not provided with surrounding exterior walls shall be the usable area under the horizontal projection of the roof or floor above.

12.0 FLOOR AREA RATIO

Floor Area Ratio (FAR) is a ratio of the gross floor area permitted on a site divided by the total net area of the site. Intensity is measured in FAR and typically describes nonresidential uses.

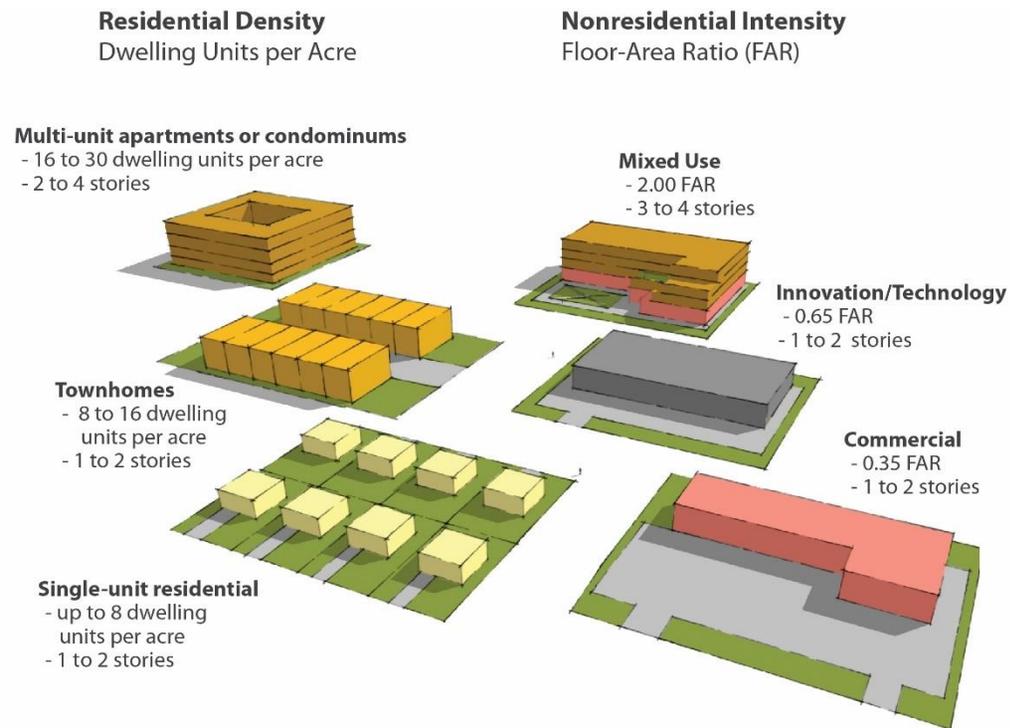


Figure 7-1



COVINA TOWN CENTER SPECIFIC PLAN